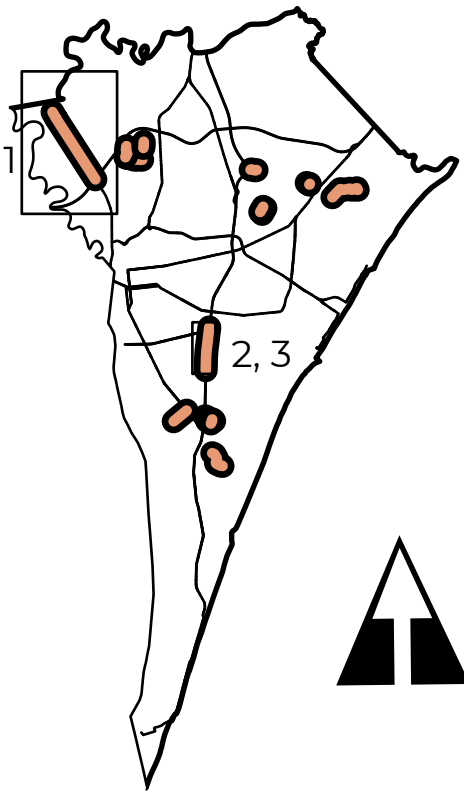
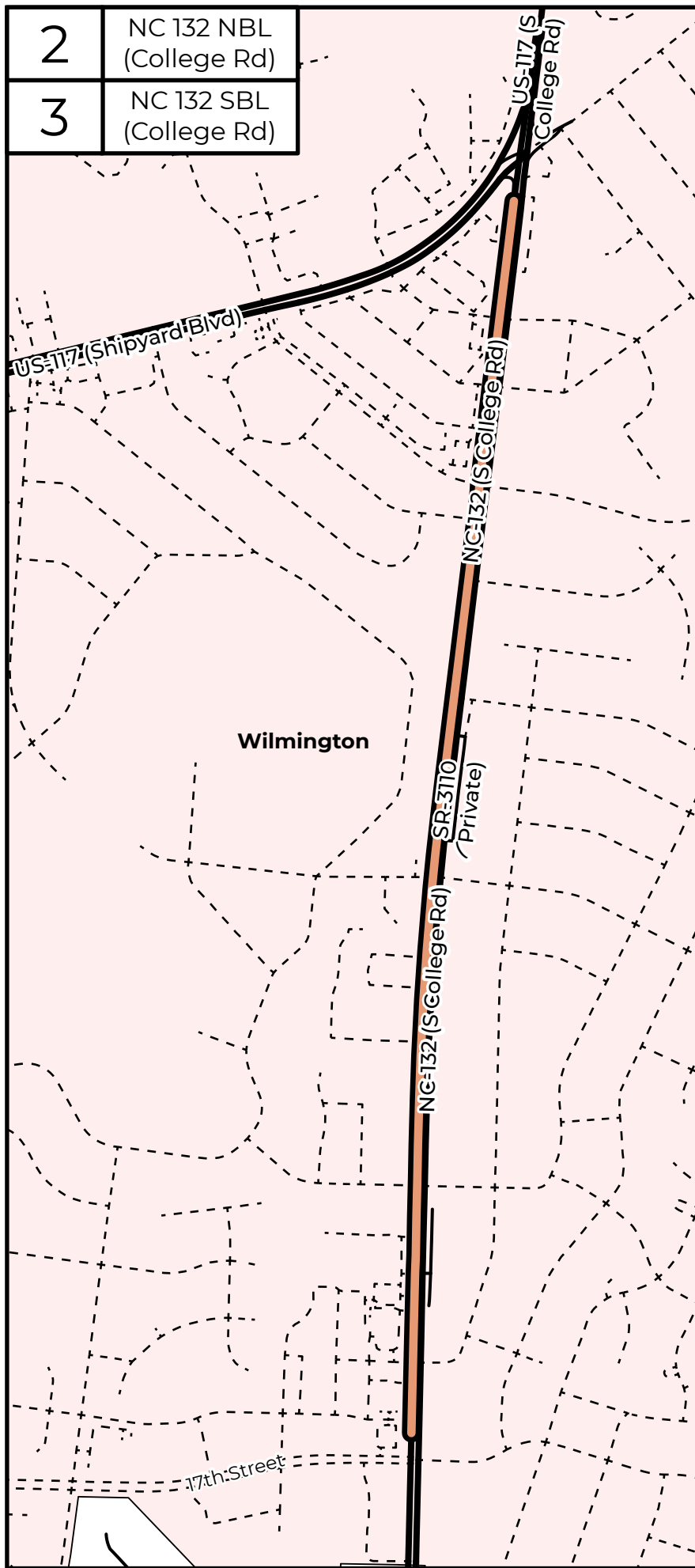
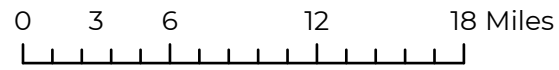


# New Hanover County 1-3

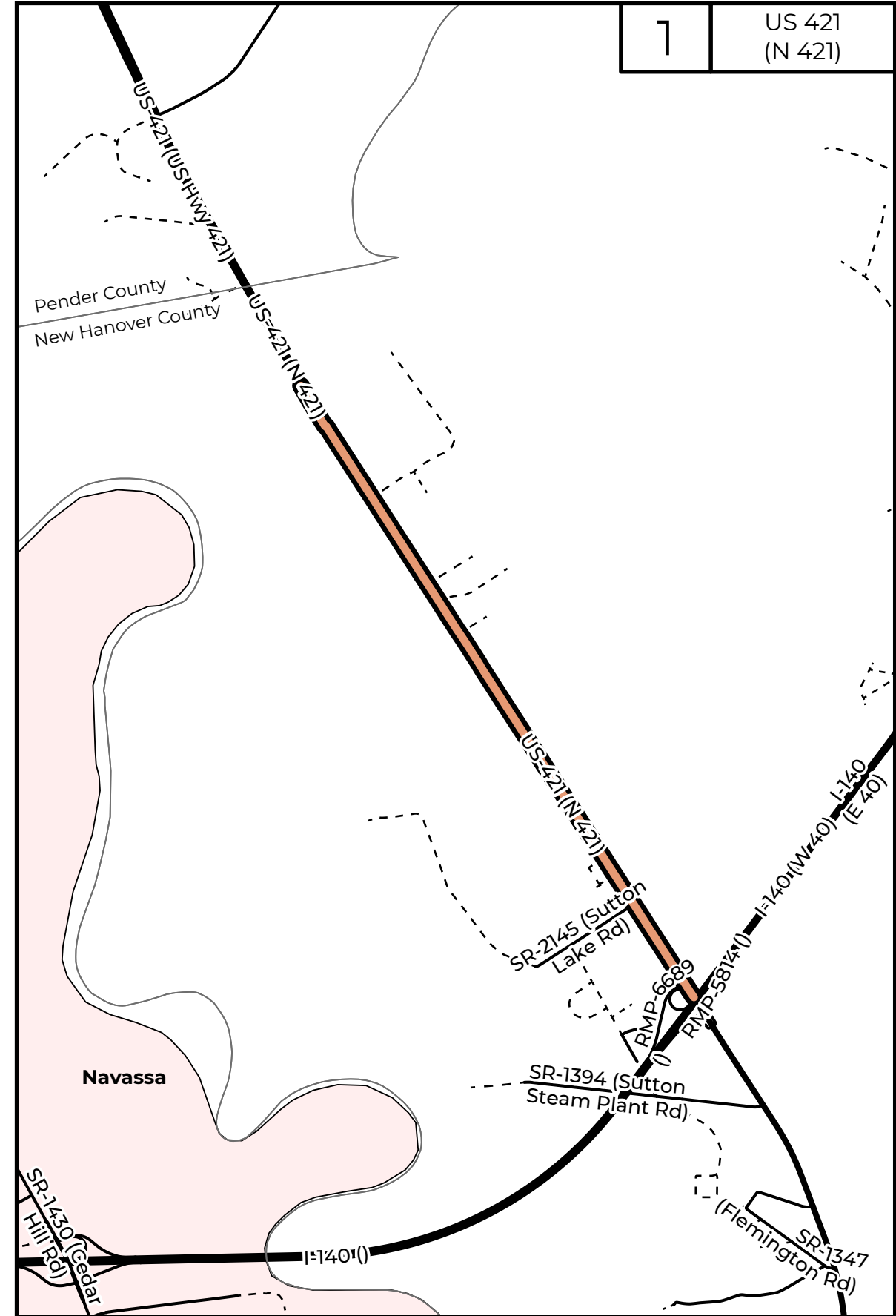


**Legend**

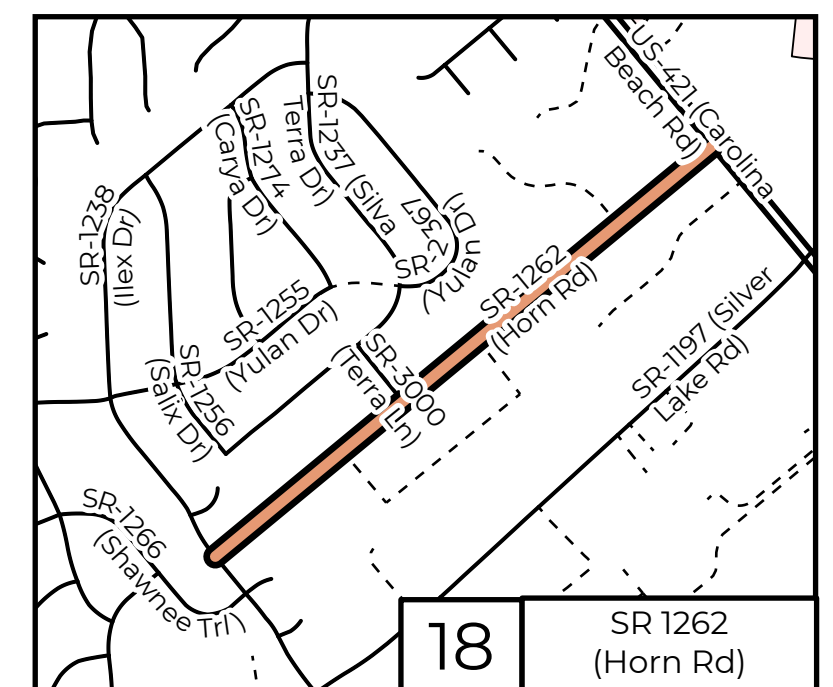
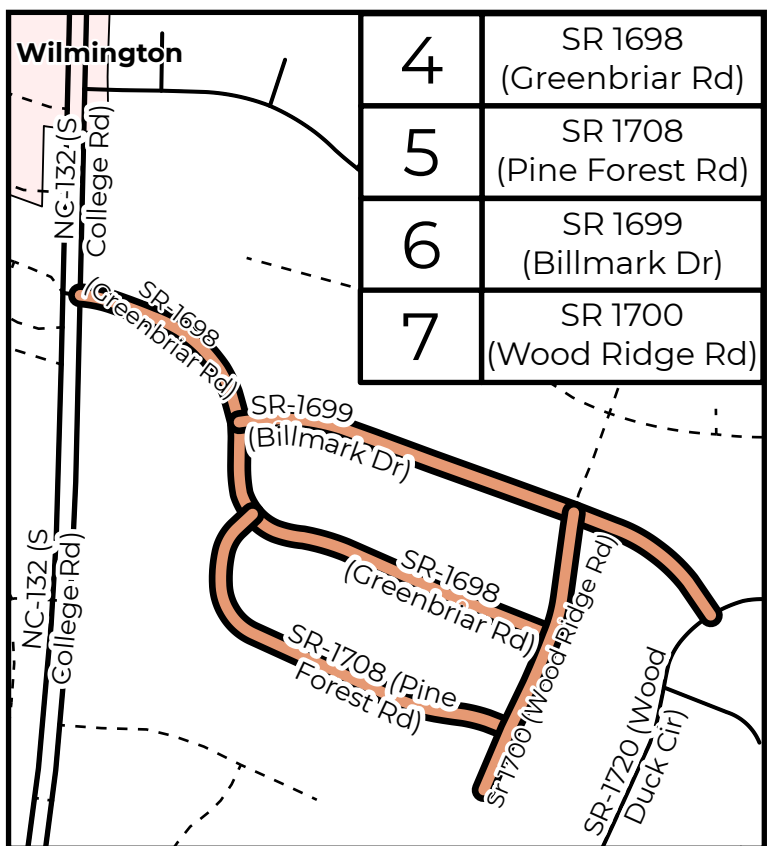
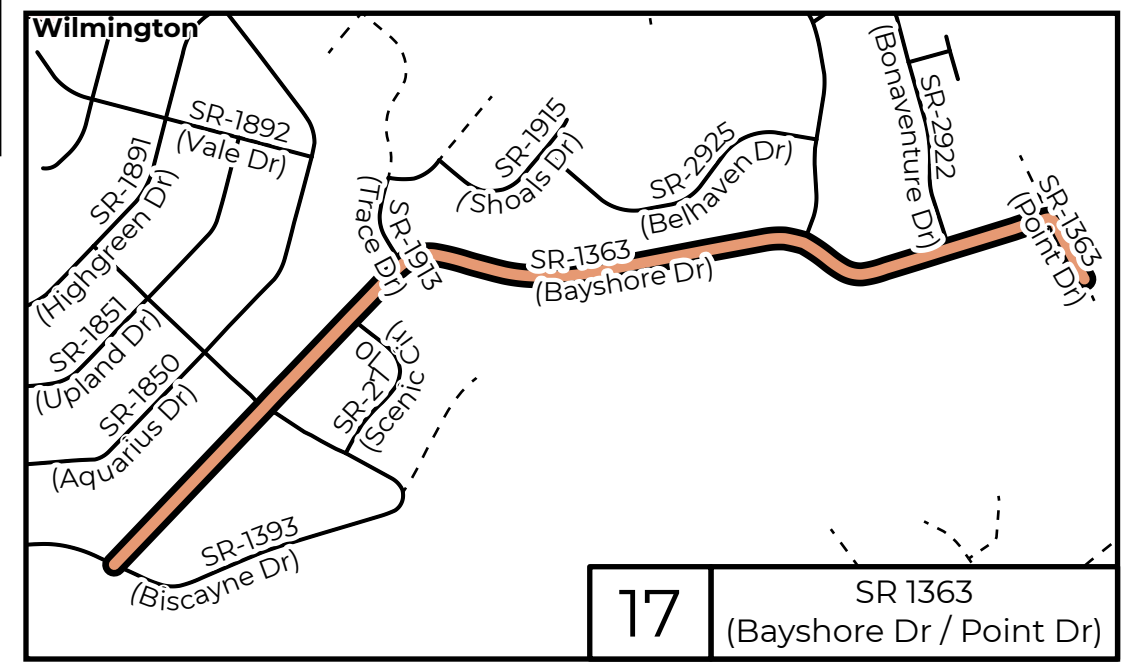
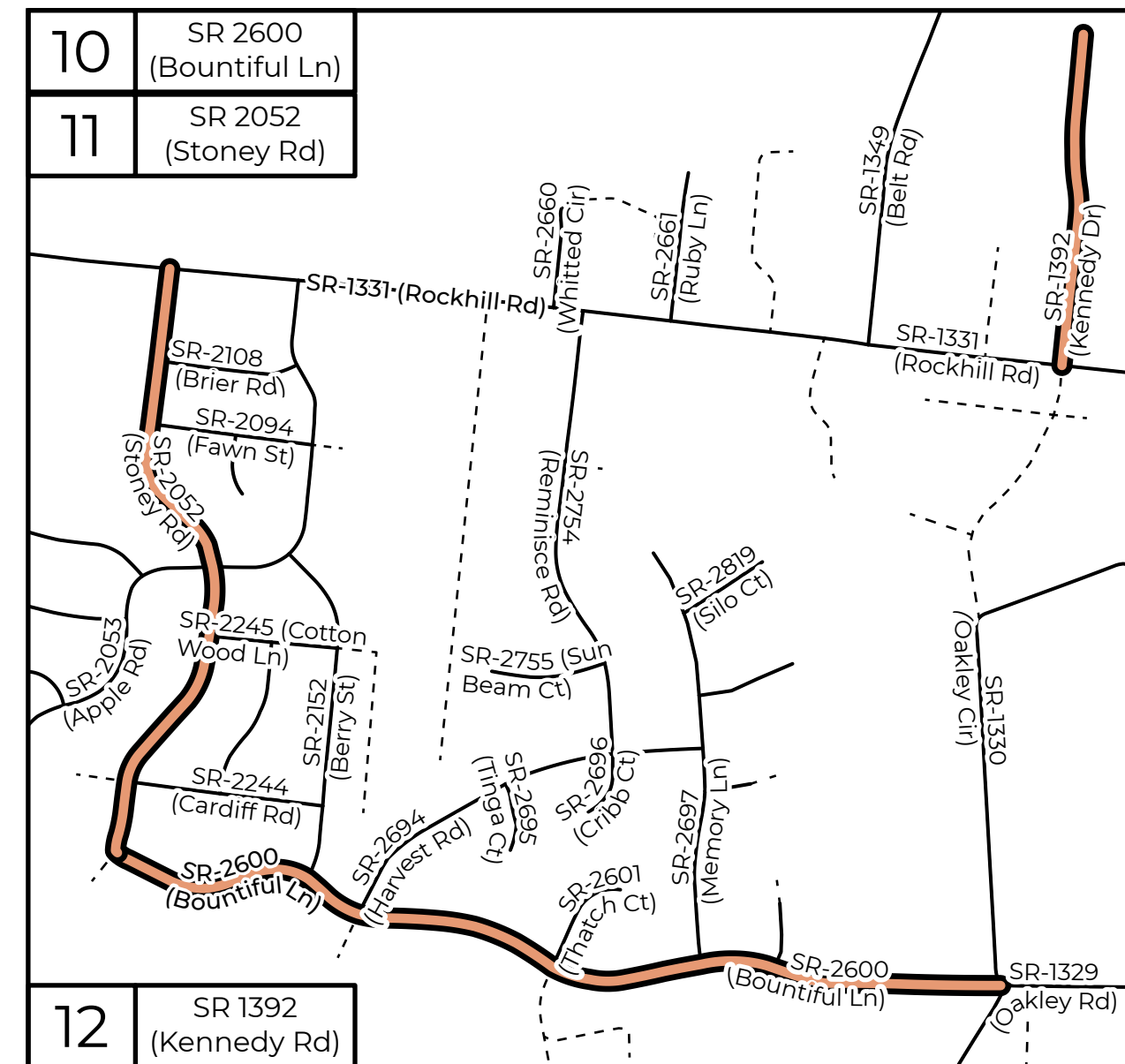
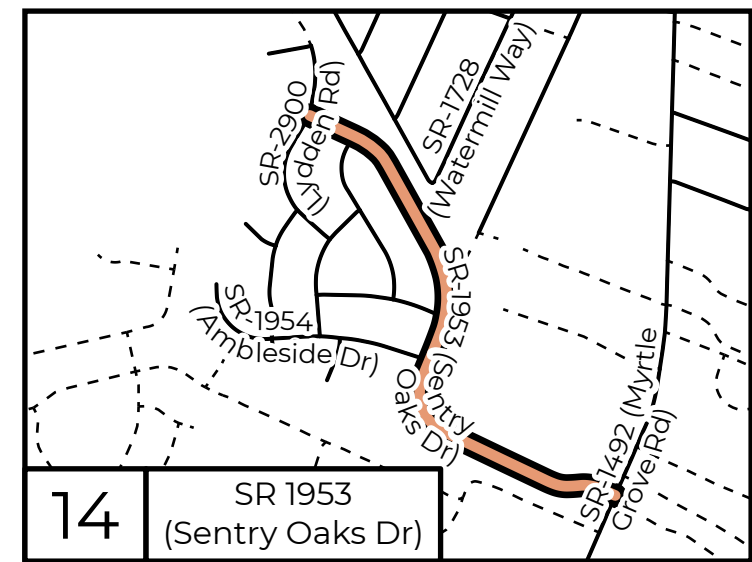
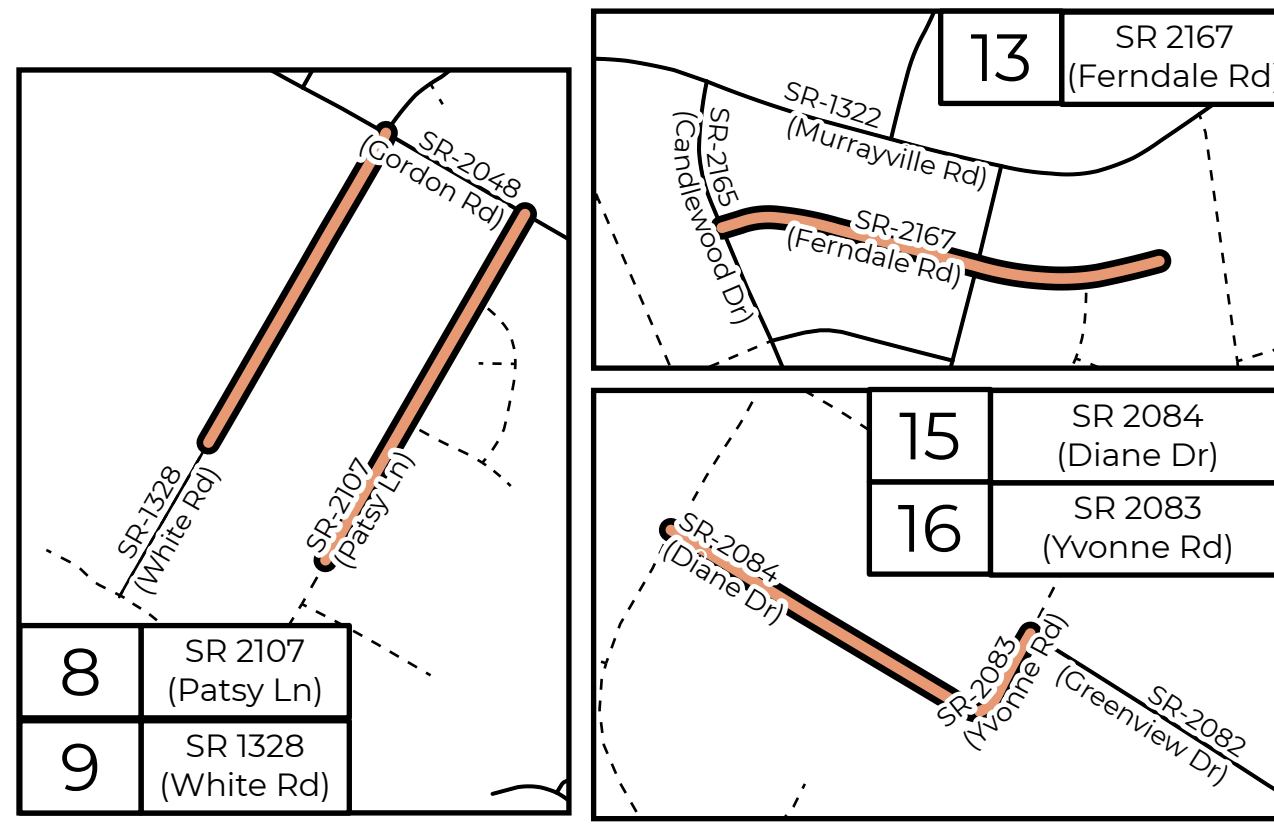
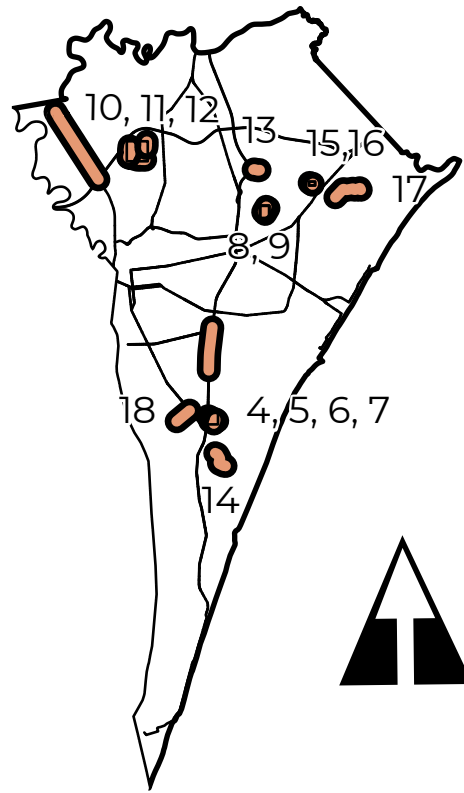
- State Maintained Road
- Bridge
- Municipality Road
- ▬ Road Section

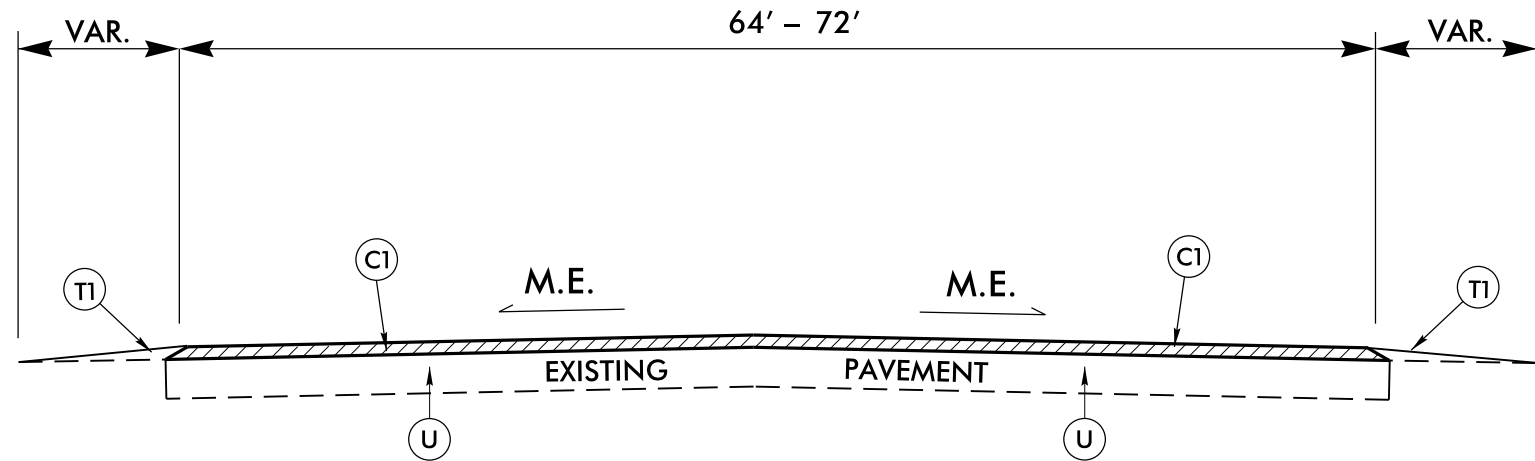


PROJECT REFERENCE NO. 2021CPT.03.17.10651, ETC.	SHEET NO. 1
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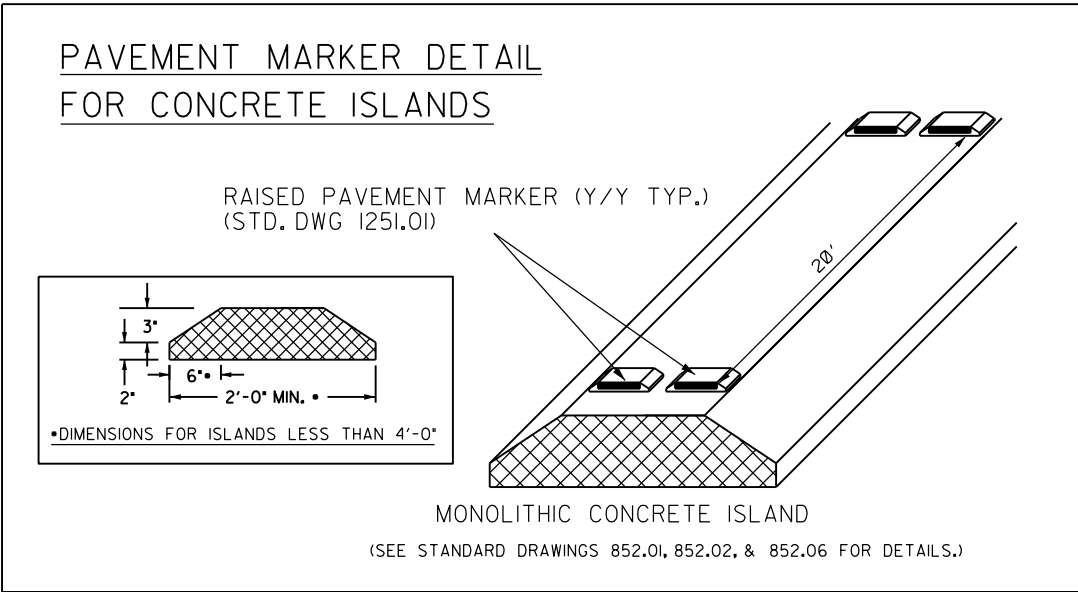
# New Hanover County 4-18





**TYPICAL SECTION NO. 1**

**MAP NO. 1  
US 421 HWY.  
MP 25.99 - MP 28.97**



PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1½" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ.YD.
C2	PROP. APPROX. 2" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ.YD.
C3	PROP. APPROX. 1½" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ.YD.
C4	PROP. APPROX. 1¼" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 137.5 LBS. PER SQ.YD.
D1	PROP. APPROX. 3" DEPTH ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C, AT AN AVERAGE RATE OF 342 LBS. PER SQ.YD.
E1	PROP. APPROX. 5" DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS. PER SQ.YD.
R1	EXISTING CONCRETE 1'-6" CURB & GUTTER
R2	EXISTING CONCRETE SHOULDER BERM GUTTER
R3	EXISTING 2'-6" CURB & GUTTER
R4	EXISTING ASPHALT CURB
R5	EXISTING CONCRETE VALLEY CURB
T1	EARTH MATERIAL (SHOULDER RECONSTRUCTION)
T2	EXISTING EARTH MATERIAL
U	EXISTING PAVEMENT
V1	MILLING ASPHALT PAVEMENT, 2" DEPTH
V2	FINE MILLING, 2" DEPTH
V3	VARIABLE MILLING 1" TO 2" DEPTH

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

SHOULDER WORK ON MAP NO. 1 - 13, 15 - 18 AS NEEDED, TO BE DETERMINED BY ENGINEER.

2018 ROADWAY ENGLISH STANDARD DRAWINGS

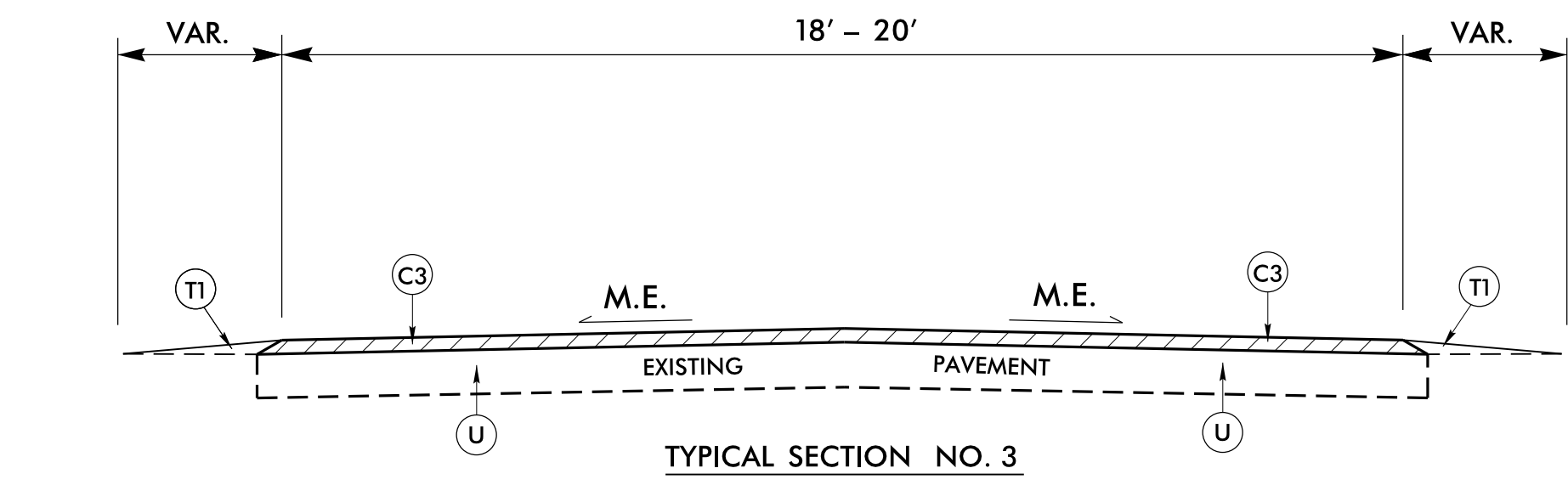
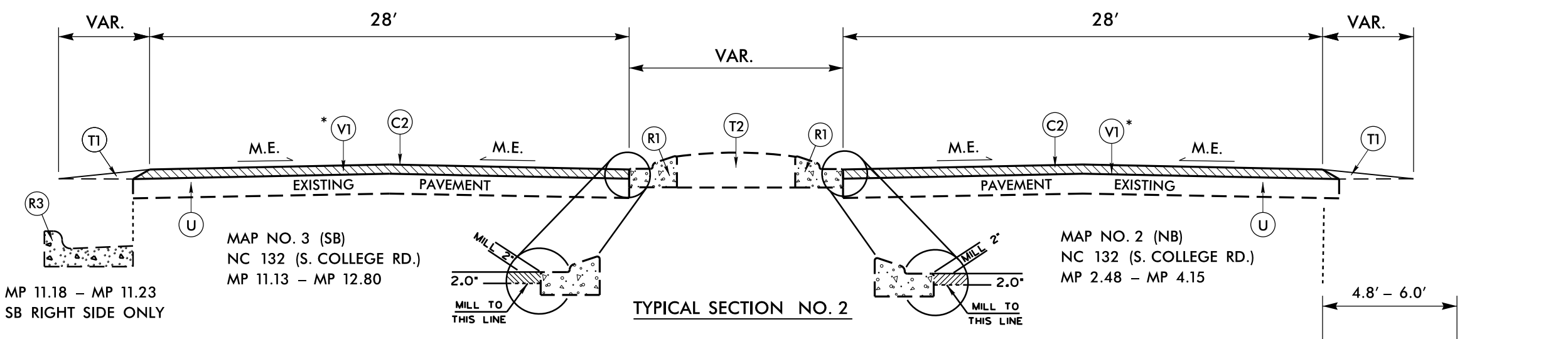
EFF.01-16-2018 REV.

The following Roadway Standards as appear in "Roadway Standard Drawings" Highway Design Branch - N.C. Department of Transportation - Raleigh, N.C., Dated January, 2018 are applicable to this project and by reference hereby are considered a part of these plans:

STD.NO.	TITLE
<b>DIVISION 8 - INCIDENTALS</b>	
846.01	Concrete Curb, Gutter and Curb & Gutter
848.01	Concrete Sidewalk
848.05	Curb Ramp - Proposed Curb & Gutter
848.06	Curb Ramp - Existing Curb & Gutter
852.01	Concrete Islands
862.01	Guardrail Placement
862.02	Guardrail Installation
<b>DIVISION 12 - PVMT MARKING, MARKERS AND DELINEATION</b>	
1205.06	Lane Drops
1205.08	Pavement Markings - Symbols & Word Messages

8/17/99  
 REVISIONS  
 08-MAP-202120402  
 S:\Division\Resurfacing\Resurfacing Data\2021\Resurfacing\NEW\_HANOVER\SPRING LET\2021CPT.03.17.10651.Rd.L.tpd.dgn  
 \$\$\$USERNAME\$\$\$

PAVEMENT SCHEDULE	
C2	2" S9.5C
C3	1½" S9.5B
D1	3" I19.0C
E1	5" B25.0C
R1	EXISTING CONC. 1' X 6" CURB
R2	EXISTING CONC. SHLD. BERM GUTTER
R3	EXISTING CONC. 2' X 6" CURB
T1	EARTH MATERIAL SHOULDER RECONST.
T2	EXISTING EARTH MATERIAL
U	EXISTING PAVEMENT
V1	MILLING ASPHALT PAVEMENT, 2" DEPTH
V2	FINE MILLING, 2" DEPTH
V3	VARIABLE MILLING 1" TO 2" DEPTH



NOTES: \* MILL A SINGLE LANE AND PAVE BACK BY THE END OF EACH WORK DAY EXCEPT IN FINE MILLING AREA

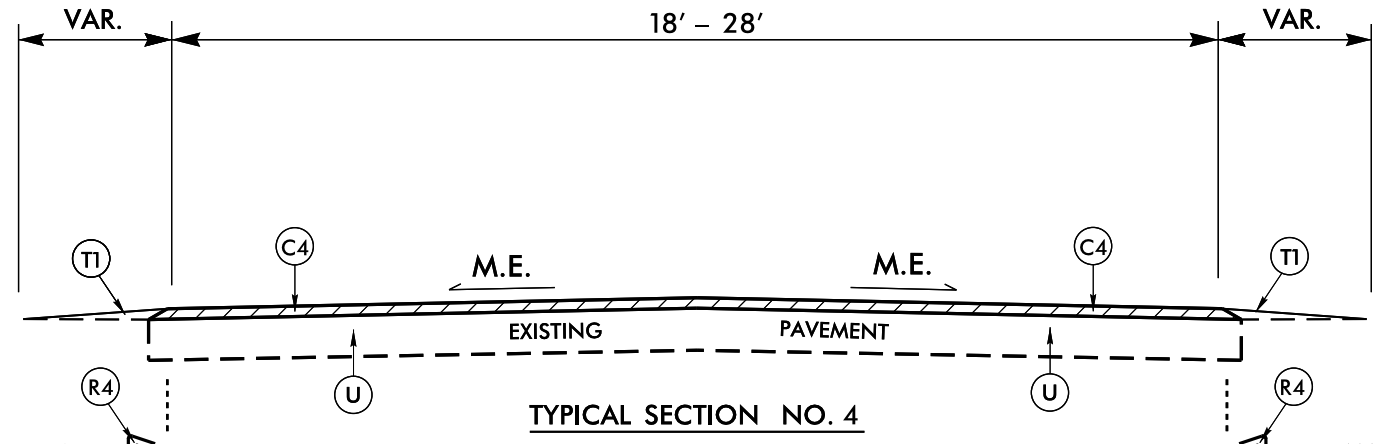
Ⓥ2 FINE MILLING LOCATIONS:  
MAP NO.2 FROM MP 3.65 - MP 4.15  
(BENTLEY DR. TO PROJECT LIMIT)

MAP NO.3 FROM MP 11.13 - MP 11.63  
(PROJECT LIMIT TO BENTLEY DR.)

Ⓥ3 USE VARIABLE MILLING TO TIE IN SECONDARY -Y- LINES OR AS DIRECTED BY ENGINEER.

REVISIONS

08-MAP-2021-20-40  
S:\Division\Resurfacing\Resurfacing Data\2021\Resurfacing\NEW\_HANOVER\_SPRING\_LET\2021CPT.03.17.10651.Rdu.txd.dgn  
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\*MAP NO. 7 & 13:

\*MAP NO. 7 & 13:

MAP NO. 4  
SR 1698 (GREENBRIAR RD.)  
MP 0.00 – MP 0.36

MAP NO. 5  
SR 1708 (PINE FOREST RD.)  
MP 0.00 – MP 0.25

MAP NO. 6  
SR 1699 (BILLMARK DR.)  
MP 0.00 – MP 0.32

\*MAP NO. 7  
SR 1700 (WOOD RIDGE RD.)  
MP 0.00 – MP 0.18

MAP NO. 10  
SR 2600 (BOUNTIFUL LN.)  
MP 0.00 – MP 0.72

MAP NO. 11  
SR 2052 (STONE RD.)  
MP 0.00 – MP 0.49

\*MAP NO. 13  
SR 2167 (FERNDALE RD.)  
MP 0.00 – MP 0.25

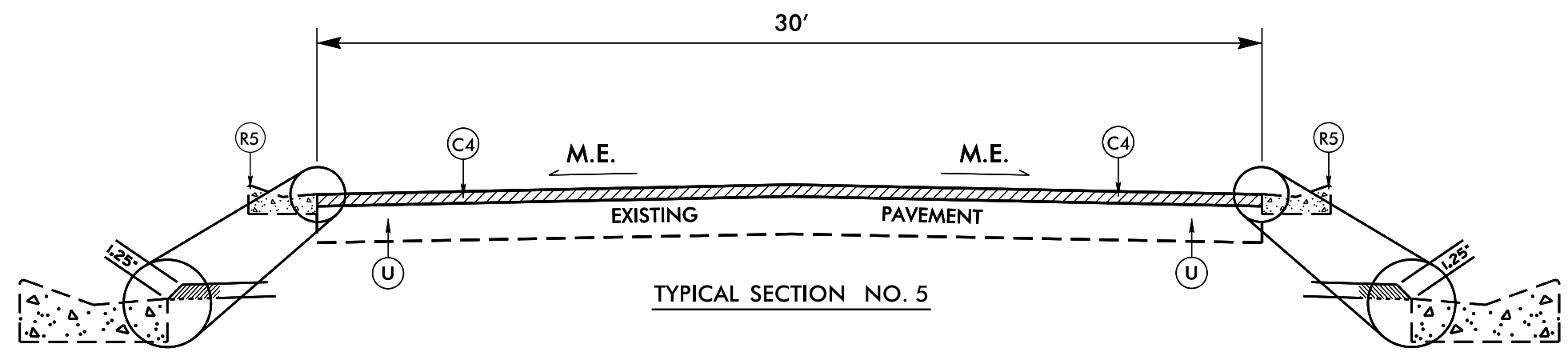
MAP NO. 15  
SR 2084 (DIANE DR.)  
MP 0.00 – MP 0.13

MAP NO. 16  
SR 2083 (YVONNE RD.)  
MP 0.00 – MP 0.03

MAP NO. 17  
SR 1363 (BAYSHORE DR./POINT DR.)  
MP 0.77 – MP 2.01

MAP NO. 18  
SR 1262 (HORN RD.)  
MP 0.00 – MP 0.70

PAVEMENT SCHEDULE	
C4	1 1/4" S9.5B
R1	EXISTING CONC. 1'X 6" CURB
R4	EXISTING ASPHALT CURB
R5	EXISTING CONC. VALLEY CURB
T1	EARTH MATERIAL SHOULDER RECONST.
U	EXISTING PAVEMENT



MAP NO. 14  
SR 1953 (SENTRY OAKS DR.)  
MP 0.00 – MP 0.79

REVISIONS

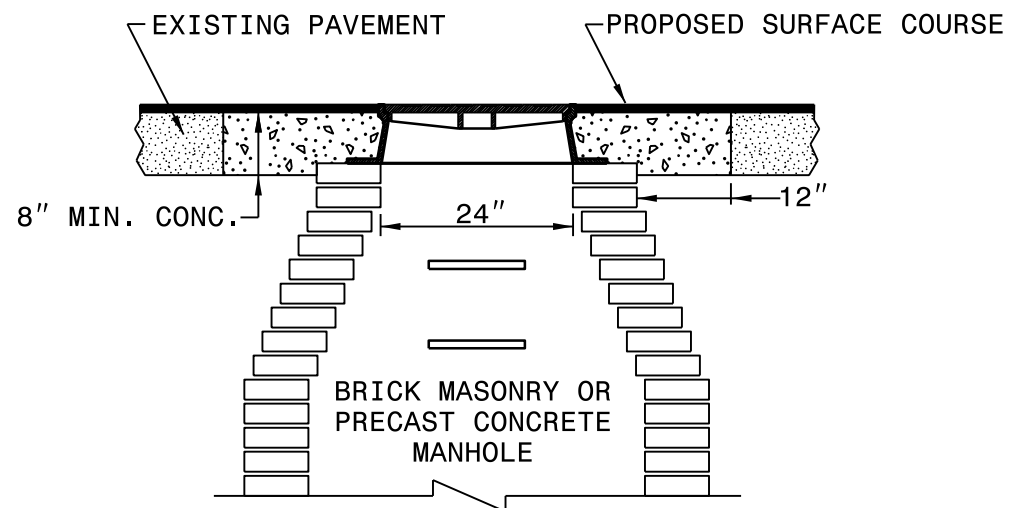
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 8/17/99

STATE OF  
 NORTH CAROLINA  
 DEPT. OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
 RALEIGH, N.C.

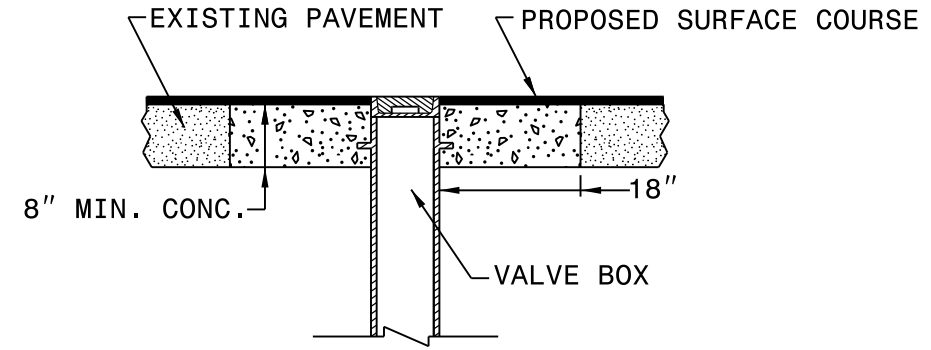
ENGLISH DETAIL DRAWING FOR  
**MANHOLE AND VALVE BOX ADJUSTMENTS**  
 SHEET 1 OF 1  
**840D55**

**GENERAL NOTES:**

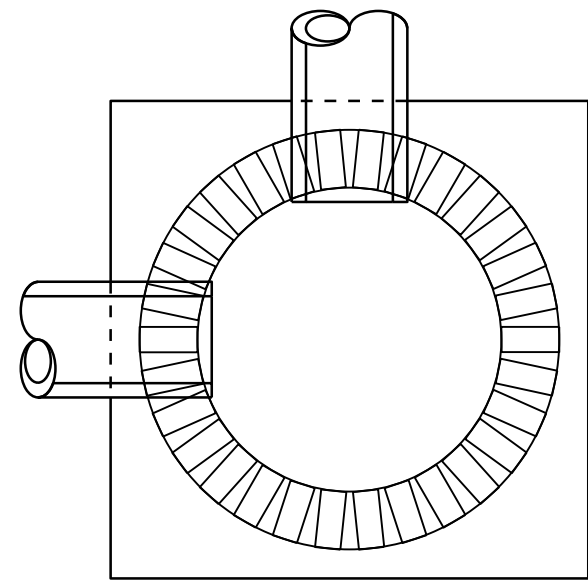
1. USE RAPID SET GROUT, MORTAR, OR CONCRETE THAT WILL TAKE FULL SET AND BECOME LOAD BEARING WITHIN SIXTY MINUTES OF PLACEMENT WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
2. REMOVE ALL FAULTY EXISTING BRICKWORK AND REPLACE WITH NEW BRICK MASONRY.
3. SHEER CUT EXCAVATION FOR THE ADJUSTMENT ON ALL SIDES.
4. FILL AREA BELOW 8" DEPTH WITH 78M OR NO. 57 CLEAN STONE.
5. MIX MORTAR TO NCDOT SPECIFICATIONS.
6. MORTAR JOINTS  $\frac{1}{2}$ " +/-  $\frac{1}{8}$ "
7. CONSTRUCT AN ASPHALT RAMP IN ACCORDANCE WITH SECTION 858-3 OF THE 2018 STANDARD SPECIFICATIONS.



**MANHOLE CONCRETE ENCASEMENT**



**VALVE BOX CONCRETE ENCASEMENT**



**ELEVATION VIEW**

PLACE BRICK ACCORDING TO ELEVATION VIEW

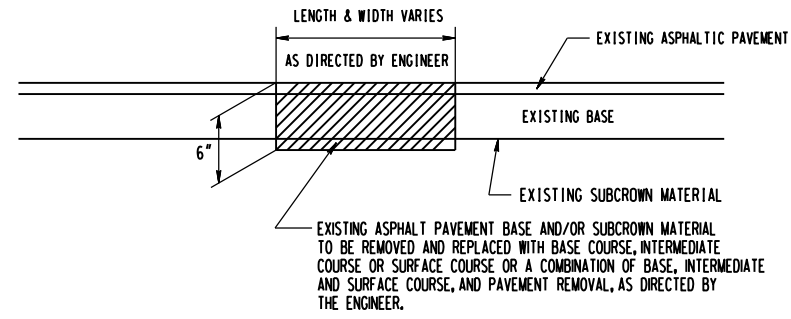
ENGLISH DETAIL DRAWING FOR  
**MANHOLE AND VALVE BOX ADJUSTMENTS**

SHEET 1 OF 1  
**840D55**

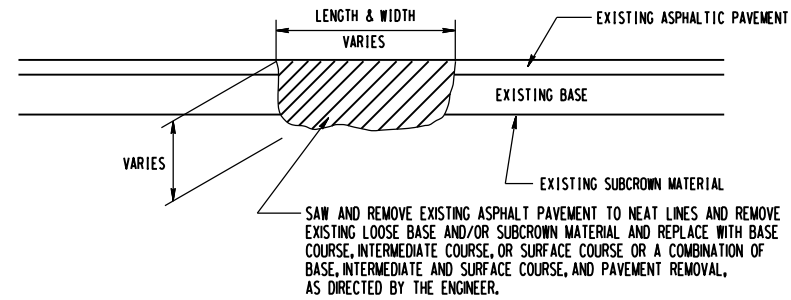
REVISIONS

28-NOV-2017 10:49  
 S:\Division Resurfacing\z Design Standards &  
 CADD\CADD\_Specs.Dwg\Tupicalis\_Curb, Etc.rxdgn  
 \$\$\$SUSPENSE\$\$\$

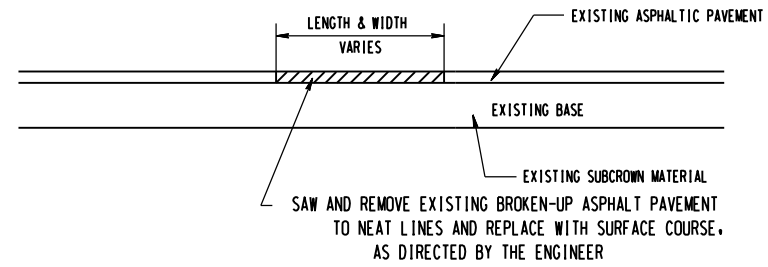
DETAILS OF REPAIRING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



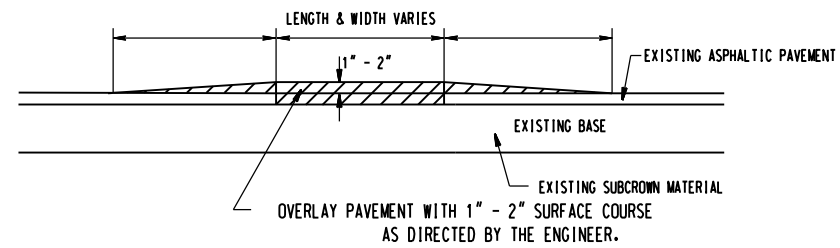
DETAIL NO. 1



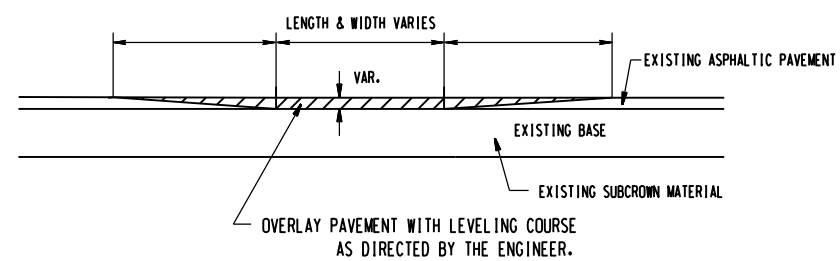
DETAIL NO. 2



DETAIL NO. 3



DETAIL NO. 4

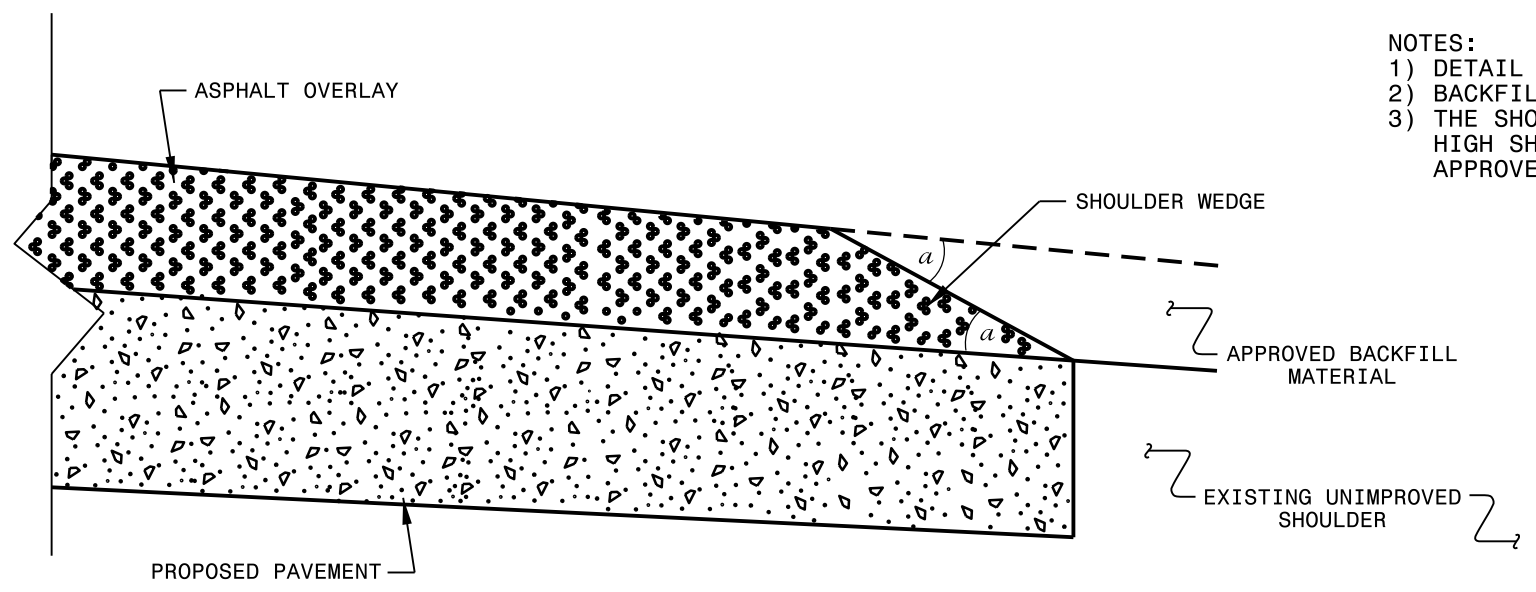


DETAIL NO. 5

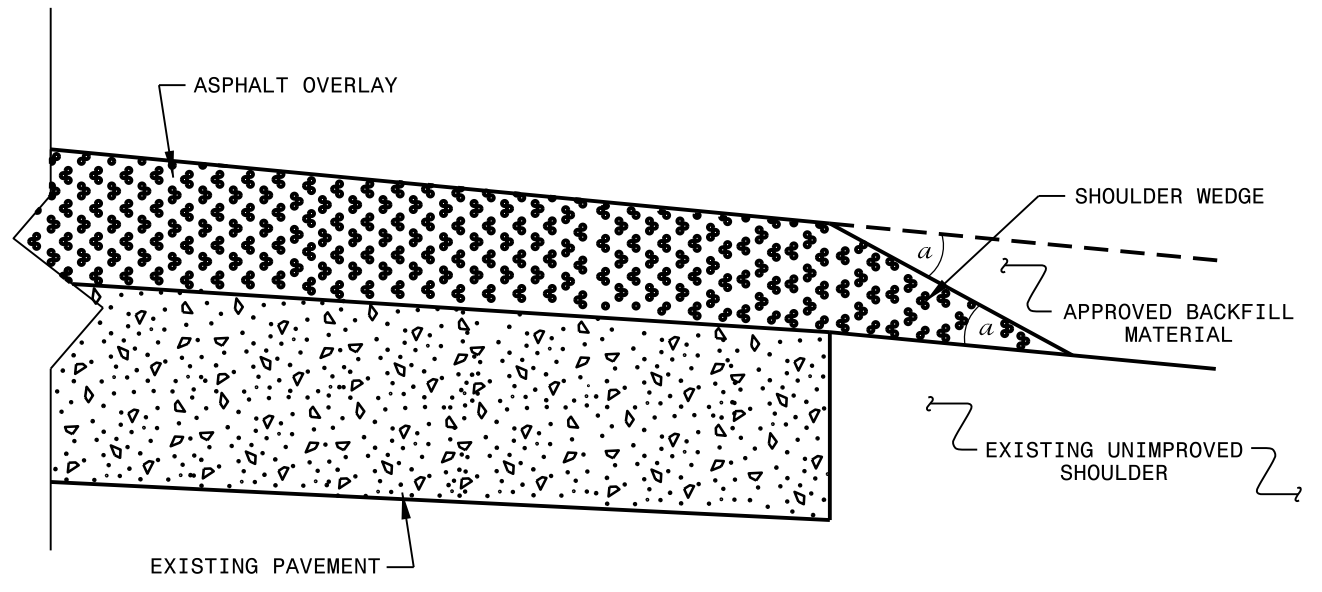
REVISIONS

20-APP-2015-1156  
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 \$\$\$SUNRISE\$\$\$

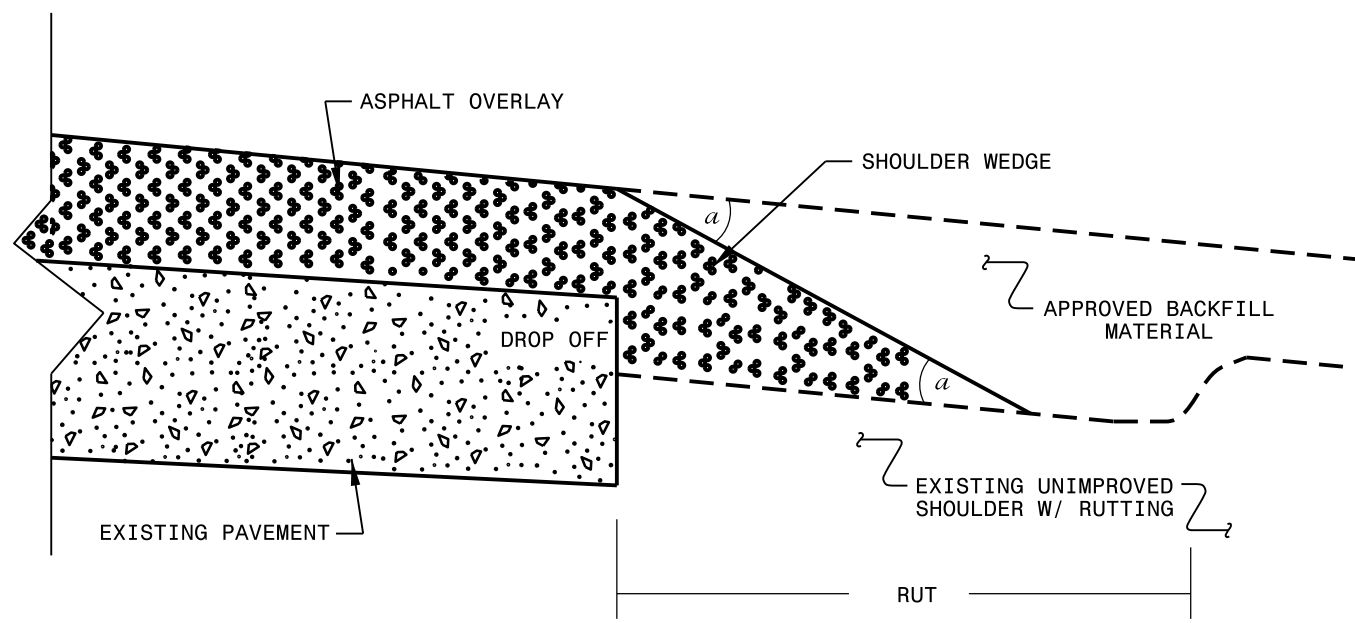
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
  - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
  - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

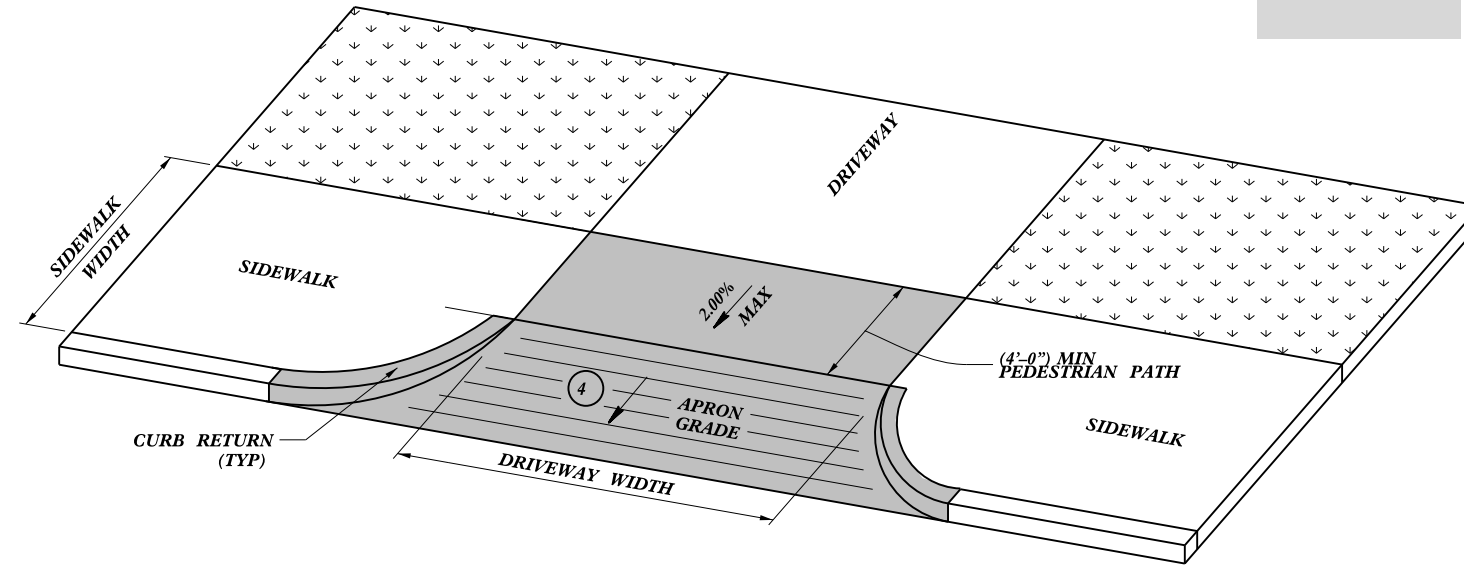
<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>			
Office 919-707-6950		FAX 919-250-4119	
<b>SHOULDER WEDGE DETAILS</b>			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn			

27 JUN 2018 13:22  
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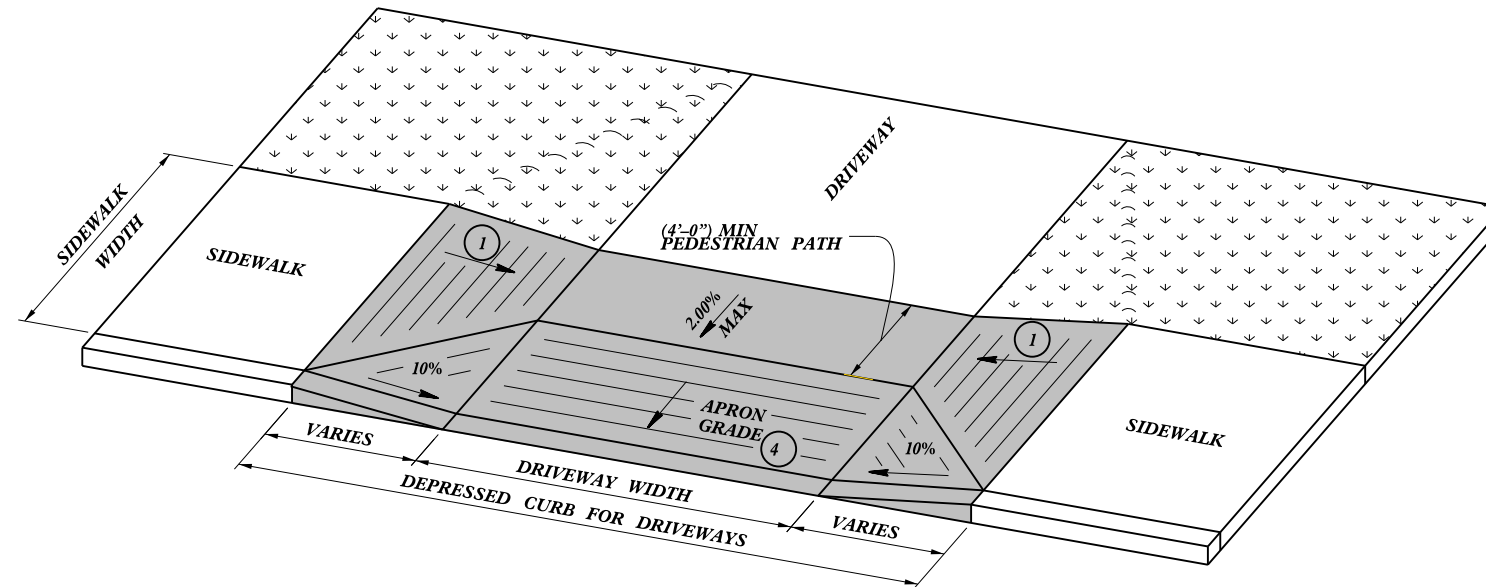


- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 4 8.00% MAX CHANGE IN GRADE BETWEEN ROAD SURFACE AND DRIVEWAY

PAY LIMITS FOR 1 CURB RAMP



**DRIVEWAY APRON**  
**OPTION 1**



**DRIVEWAY APRON**  
**OPTION 2**

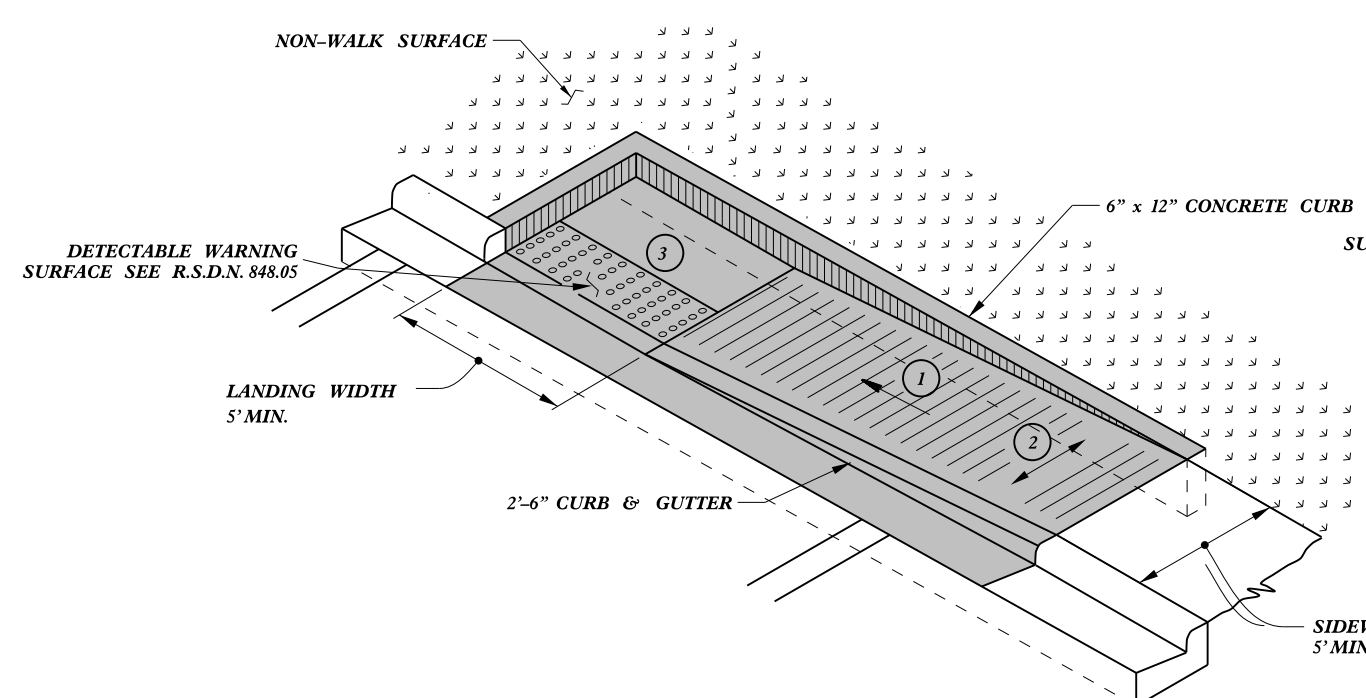
-SEE ROADWAY DETAIL DRAWING 848.05 FOR DETECTABLE WARNING SURFACE AND FOR RAMP NOTES.

-SEE ROADWAY STANDARD DRAWING 848.02 FOR CONCRETE DRIVEWAYS.

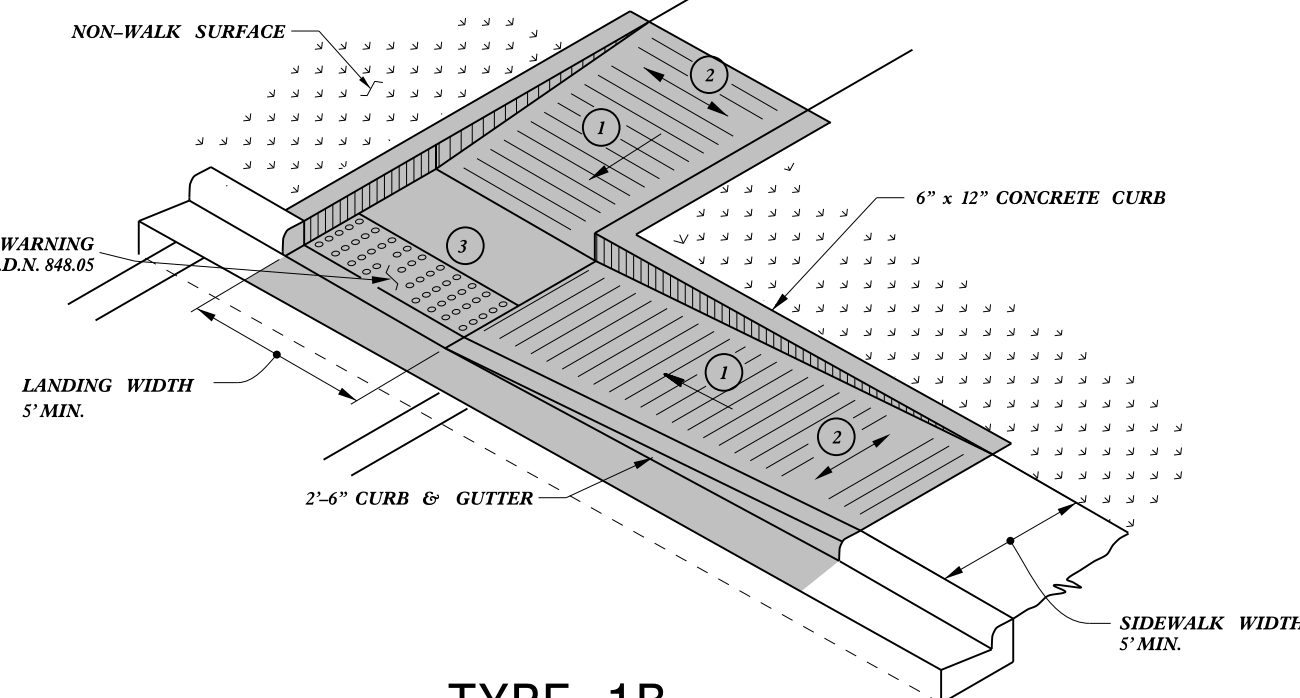
<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950	FAX 919-250-4119
<b>CURB RAMPS</b>	
@ DRIVEWAY OPENINGS	
ORIGINAL BY: J.S. HOWERTON	DATE: 7/7/11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
FILE SPEC: sstds/2012CurbRamp/CurbRampDetails.dwg	

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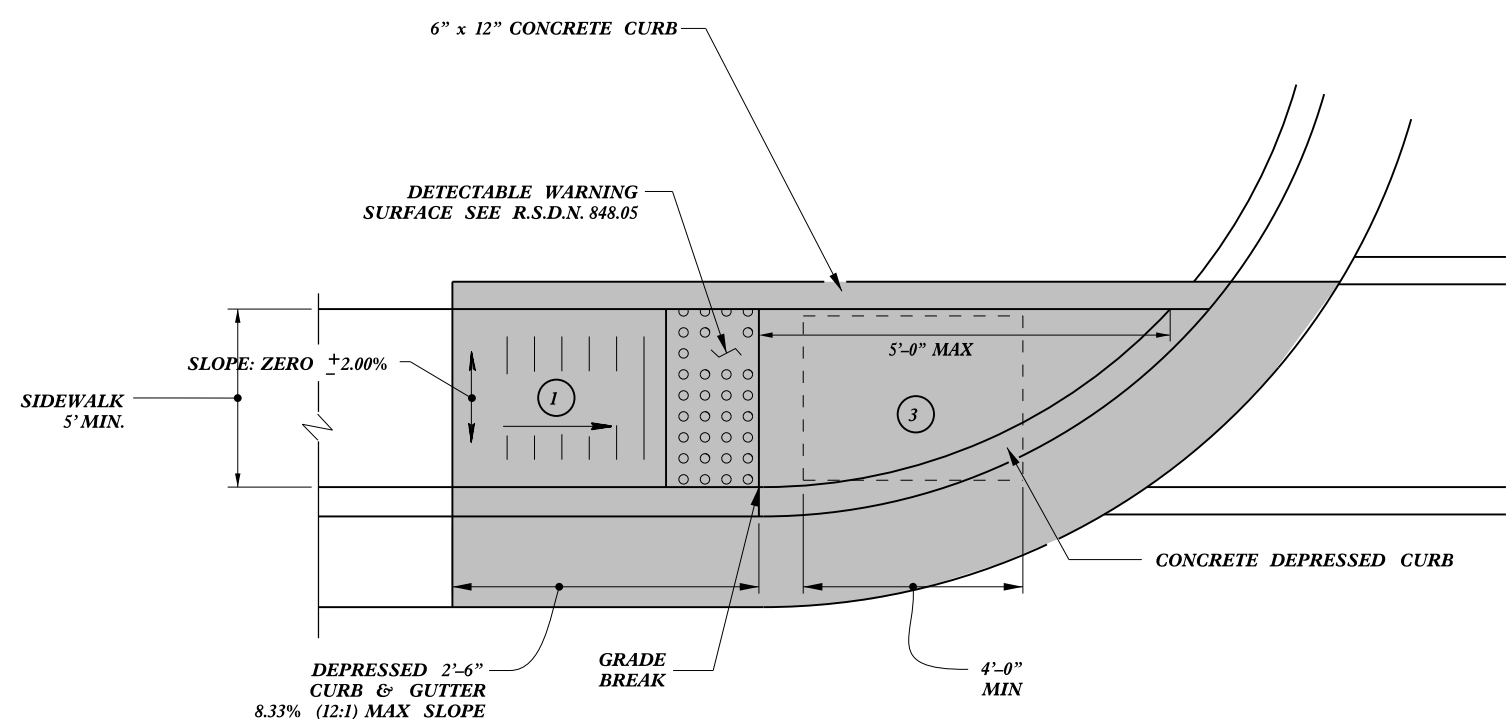
**TYPE 1A**



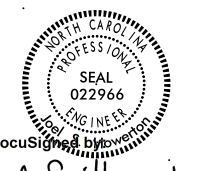
**TYPE 1B**

**PAY LIMITS FOR 1 CURB RAMP**

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.



**TYPE 1**



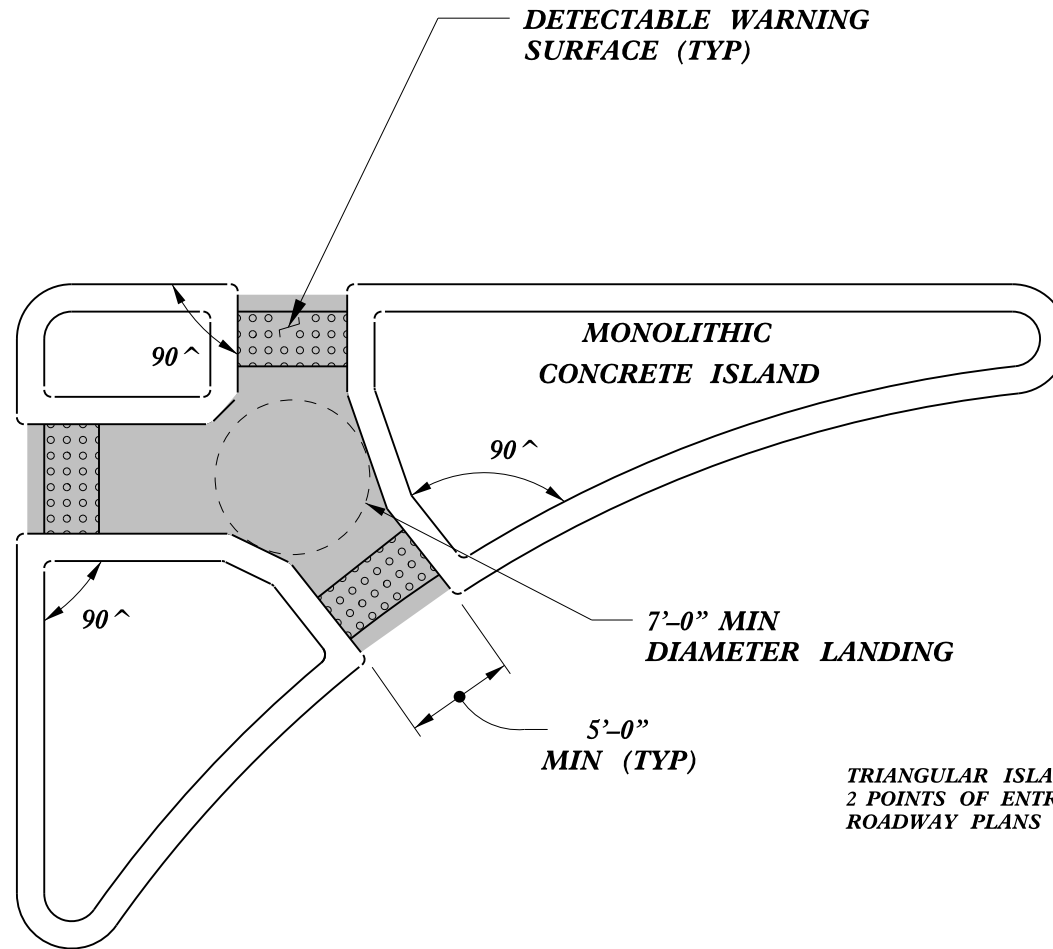
DocuSigned by:  
**Joel S. Howerton**  
 449E8E25522144F...

11/18/2015

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

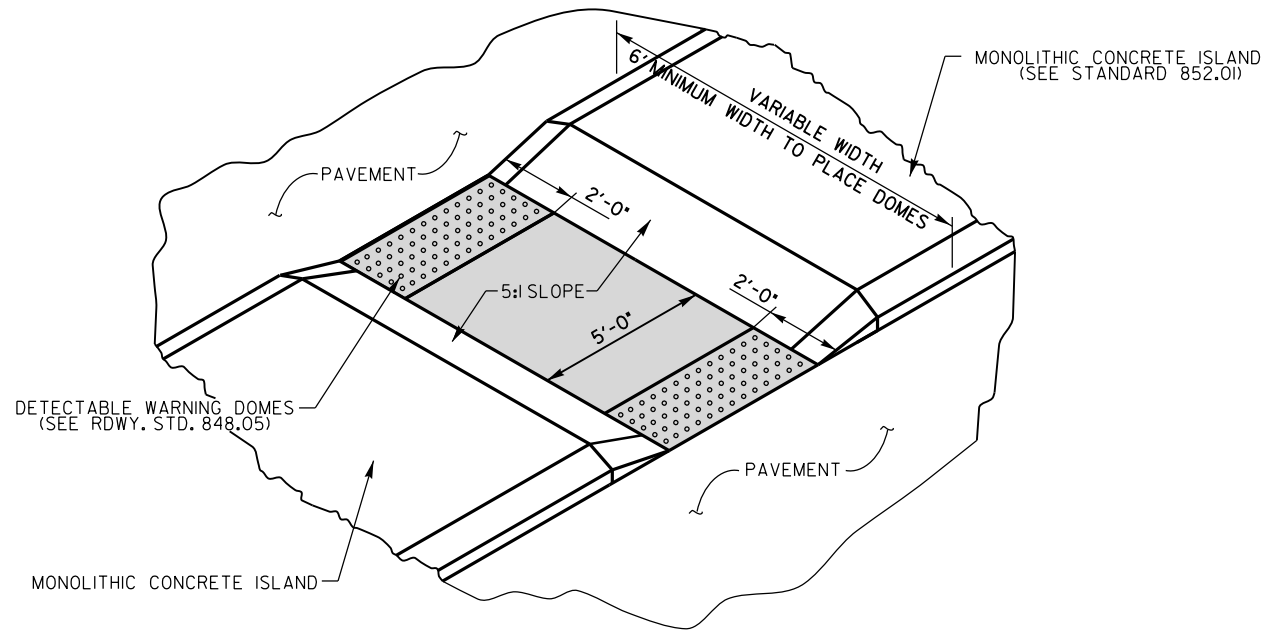
<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950	FAX 919-250-4119
<b>CURB RAMPS</b>	
Directional Ramps	
ORIGINAL BY: J.S. HOWERTON	DATE: 7/7/11
MODIFIED BY: _____	DATE: _____
CHECKED BY: _____	DATE: _____
FILE SPEC. :stds/2012CurbRamp/CurbRampDetails.dwg	

PAY LIMITS FOR 2 OR 3 CURB RAMPS  
(CALCULATE BASED ON NUMBER OF  
SETS OF TRUNCATED DOMES)

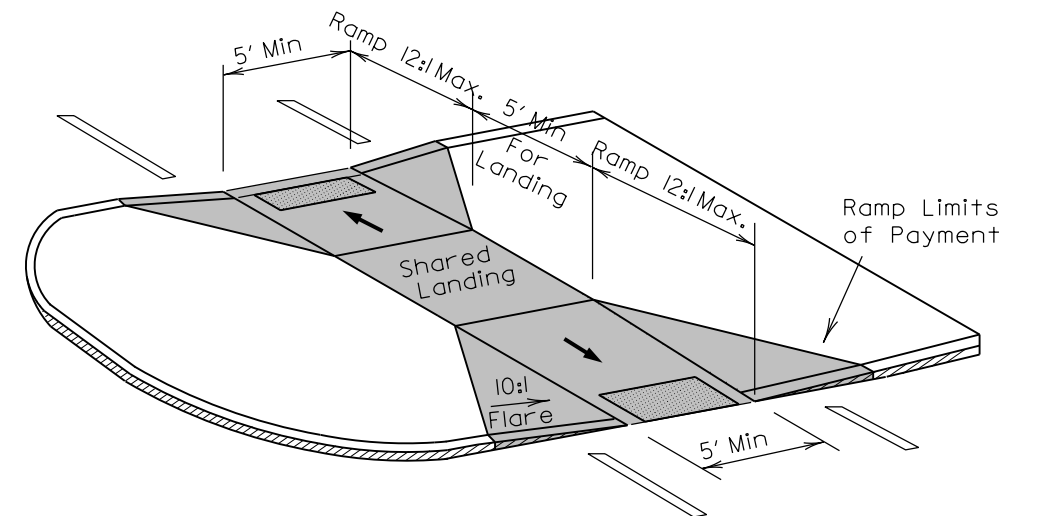


TRIANGULAR ISLANDS MAY BE CONSTRUCTED WITH ONLY  
2 POINTS OF ENTRY AND EXIT AS SHOWN IN THE  
ROADWAY PLANS OR AS DIRECTED BY THE ENGINEER.

**TRIANGULAR ISLAND  
WITH CUT THROUGH**



**MEDIAN ISLAND  
WITH CUT THROUGH**



**MEDIAN ISLAND  
CURB RAMPS**

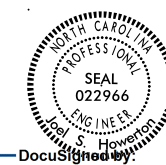
DOCUMENT NOT CONSIDERED FINAL  
UNLESS ALL SIGNATURES COMPLETED

**CONTRACT STANDARDS  
AND DEVELOPMENT UNIT**  
Office 919-707-6950 FAX 919-250-4119

**CURB RAMPS**

Median or Turn Lane Islands

ORIGINAL BY: J.S. HOWERTON DATE: 7/7/11  
MODIFIED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
FILE SPEC: stds/2012CurbRamp/CurbRampDetails.dwg

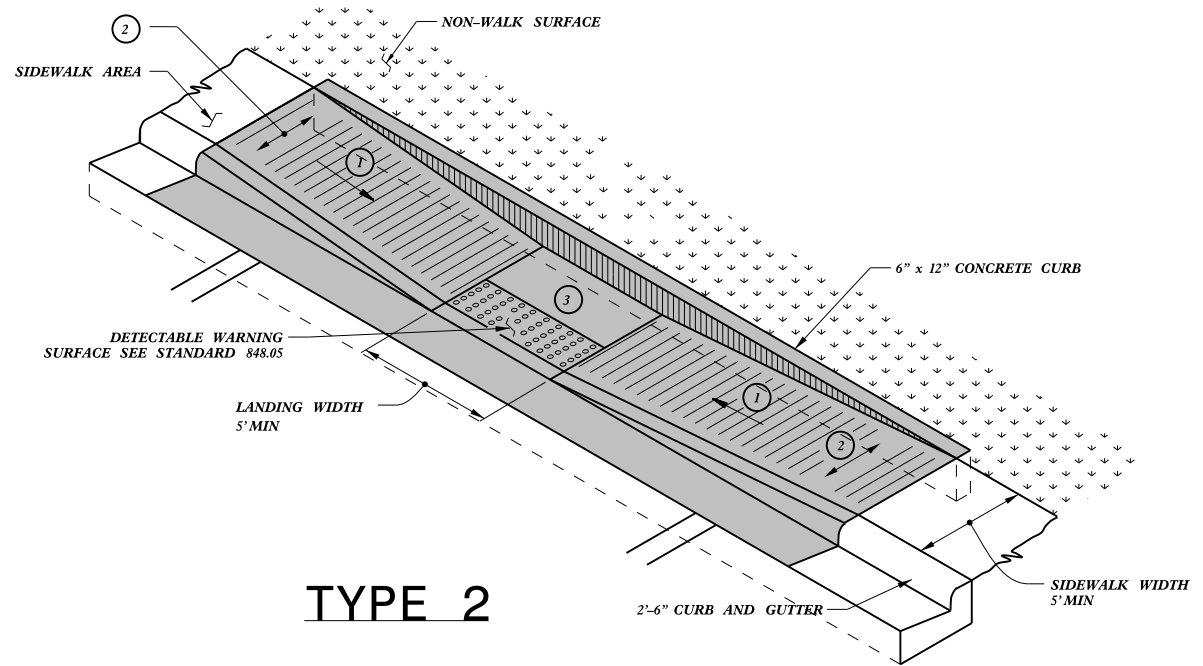


DocuSign  
*Joel S. Howerton*

11/18/2015

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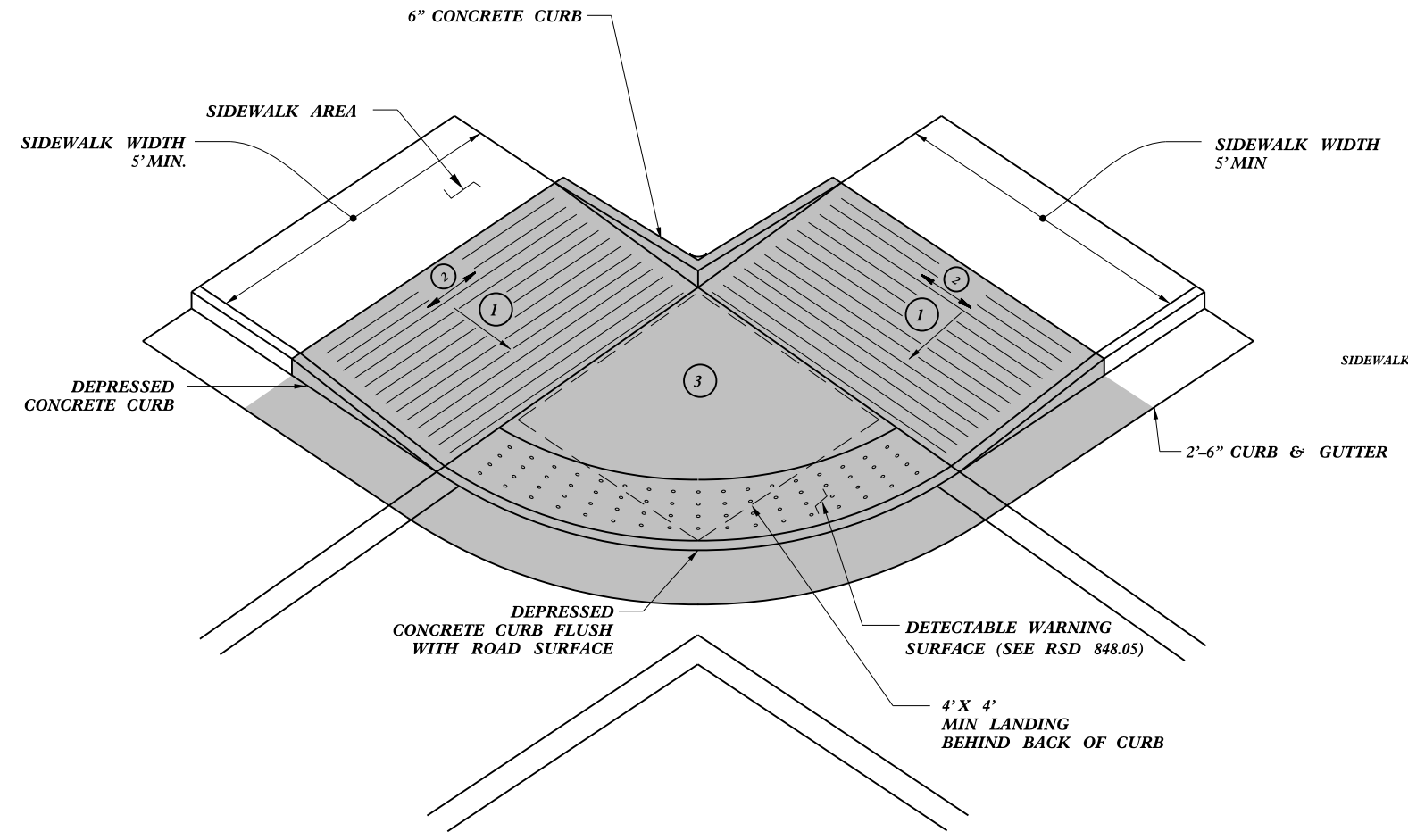
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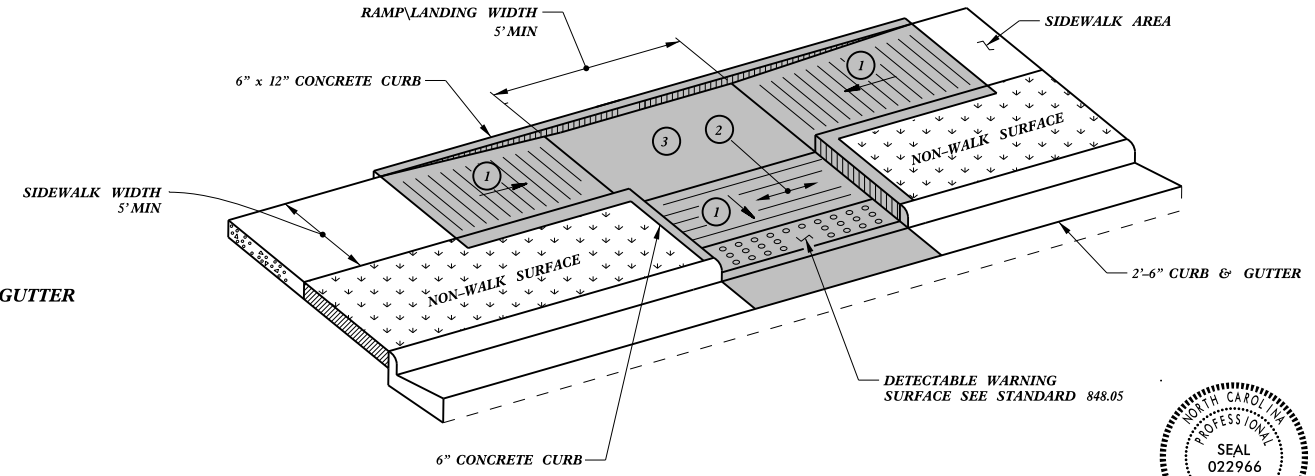
**TYPE 2**

PAY LIMITS FOR 1 CURB RAMP

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.



**TYPE 2A**



**TYPE 3**

11/18/2015



DocuSign  
 Joel S. Howerton  
 449E8E25522144E

DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

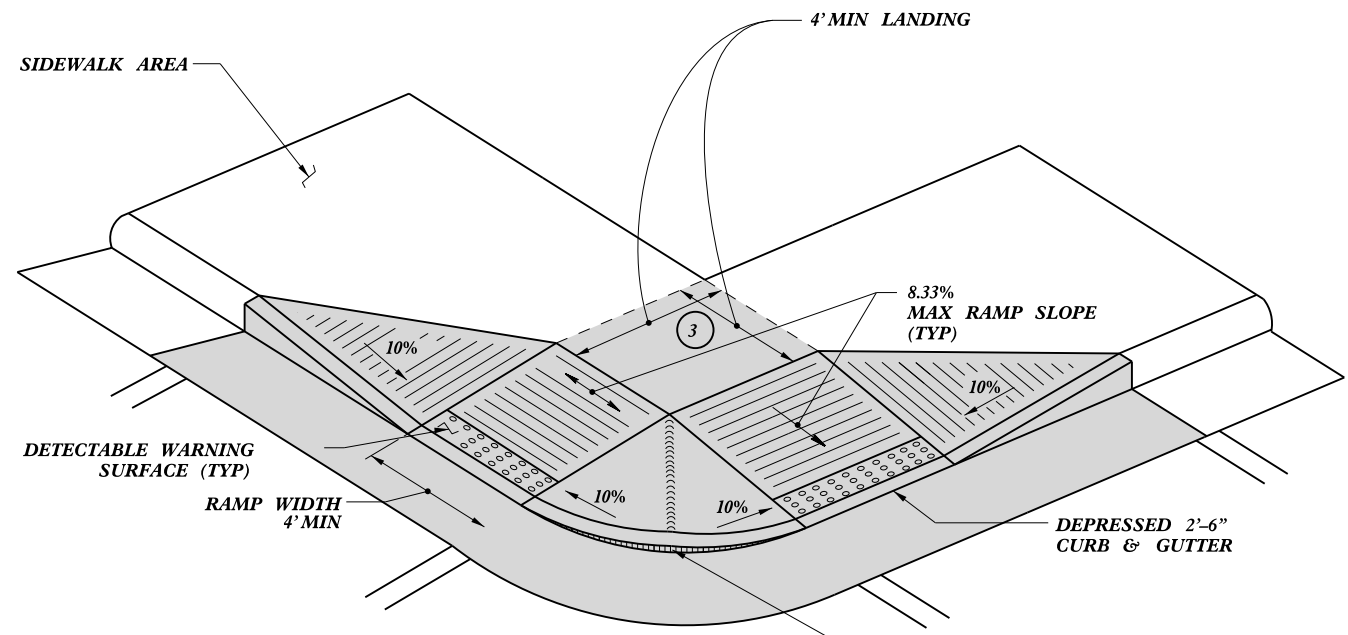
CONTRACT STANDARDS AND DEVELOPMENT UNIT  
 Office 919-707-6950 FAX 919-250-4119

**CURB RAMPS**  
 Parallel Ramps

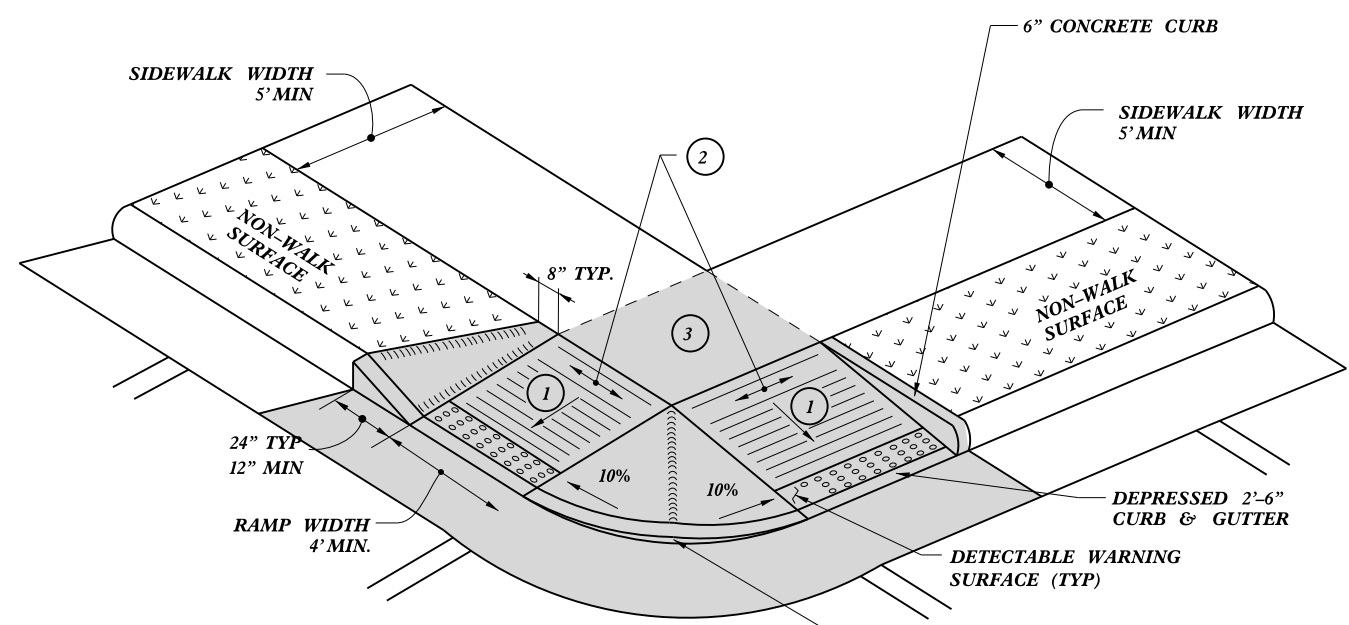
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 CHECKED BY: DATE:  
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REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

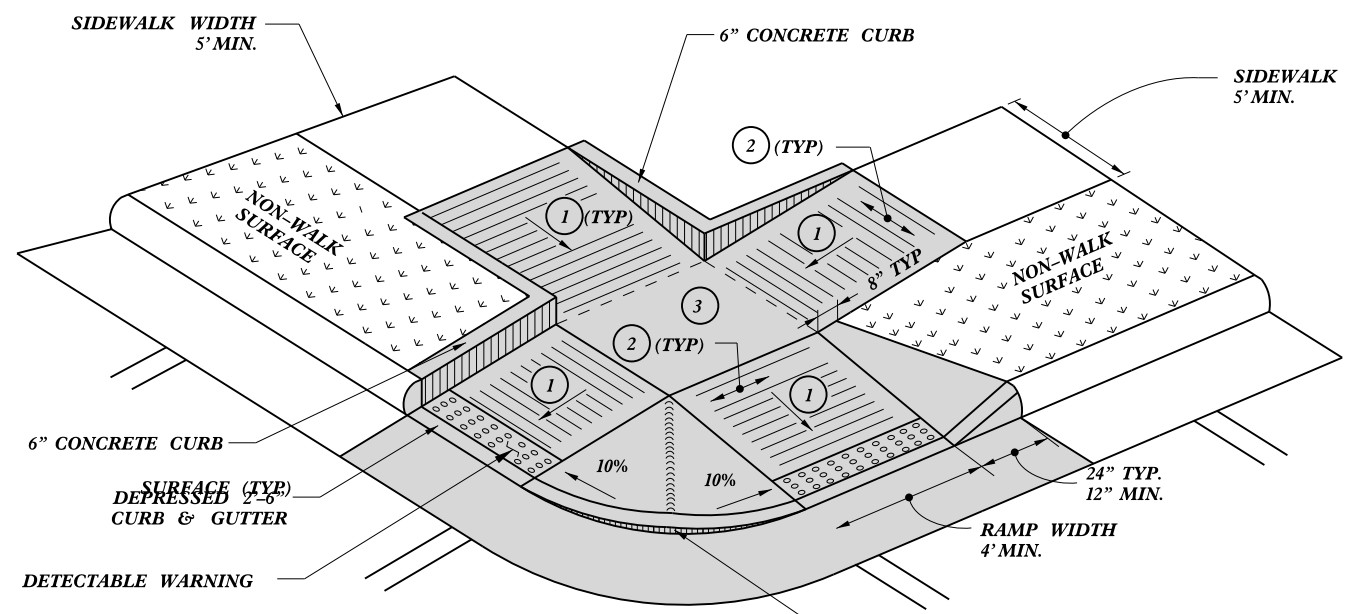
5/14/99



**TYPE 4**



**TYPE 4A**



**TYPE 5**

**PAY LIMITS FOR 2 CURB RAMPS**

- 1 8.33% (12:1) MAX RAMP SLOPE
- 2 CROSS SLOPE: 2.00%
- 3 CURB RAMPS REQUIRE A (4'-0") MINIMUM LANDING WITH A MAXIMUM CROSS SLOPE AND LONGITUDINAL SLOPE OF 2.00% WHERE PEDESTRIANS PERFORM TURNING MANEUVERS. SLOPE TO DRAIN TO CURB.

DocuSigned by:  
**Joel S Howerton**  
449E8E25522144F...



11/18/2015

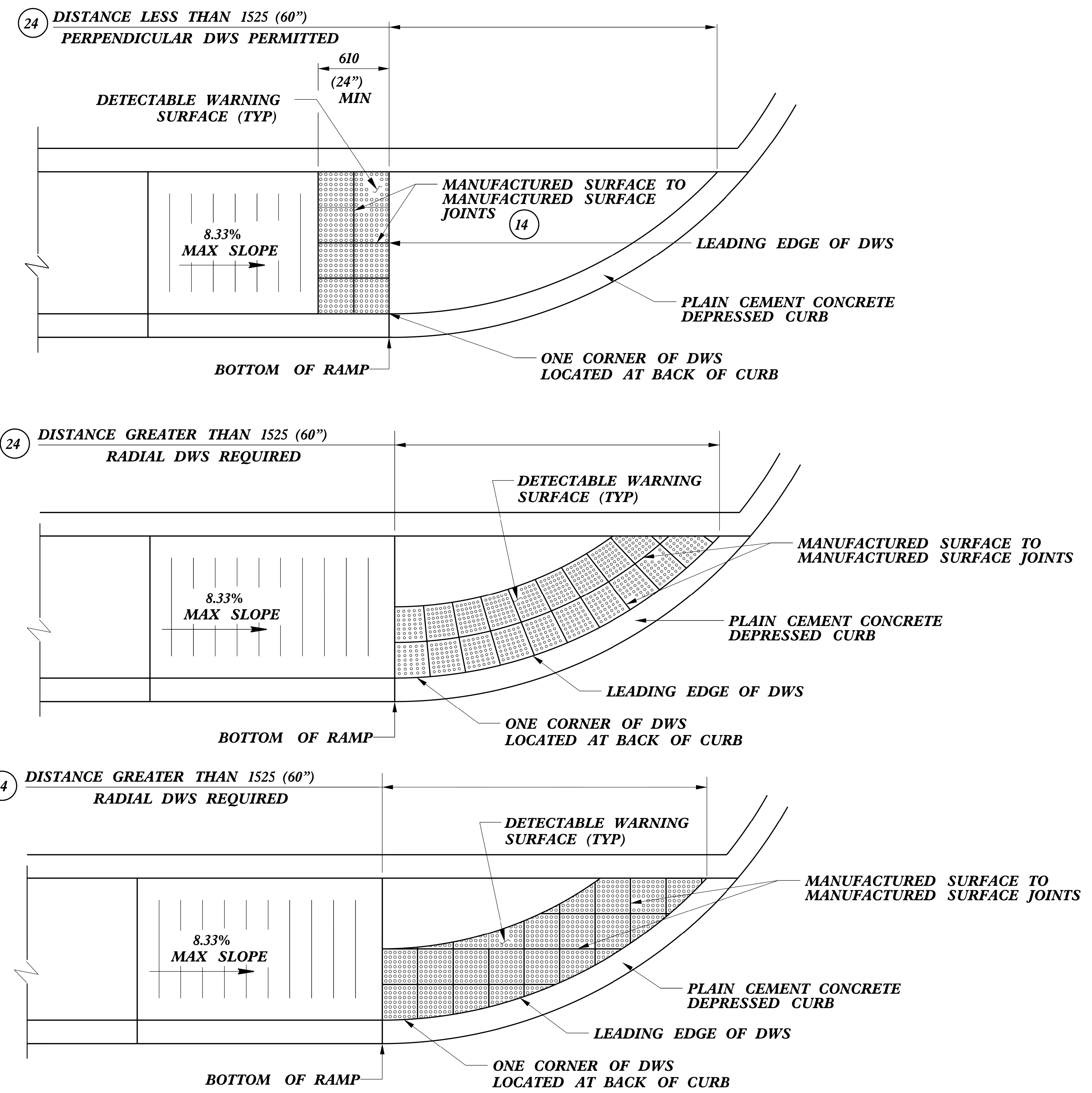
DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>	
Office 919-707-6950	FAX 919-250-4119
<b>CURB RAMPS</b>	
Shared Landing	
ORIGINAL BY: J.S. HOWERTON	DATE: 7/7/11
MODIFIED BY:	DATE:
CHECKED BY:	DATE:
FILE SPEC: stds/2012CurbRamp/CurbRampDetails.dwg	

REFER TO ROADWAY STANDARD DRAWING NUMBER 848.05 SHEET 3 OF 3 FOR ALL RAMP NOTES

5/14/1999

# TRUNCATED DOMES AT RADIUS DETAIL



**DETECTABLE WARNING SURFACE (DWS)**  
**ON CURVED SURFACES**

REVISIONS

14 DEC 2015 09:46:46 USER: J... NDER\SR-5001CB...40924.1.F78.S1.dwg Point Loop MUP\ROADWAY\Proj\Plan\_Sheets\3CR.PE\_PSH.2F.dgn  
 8/17/2015

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

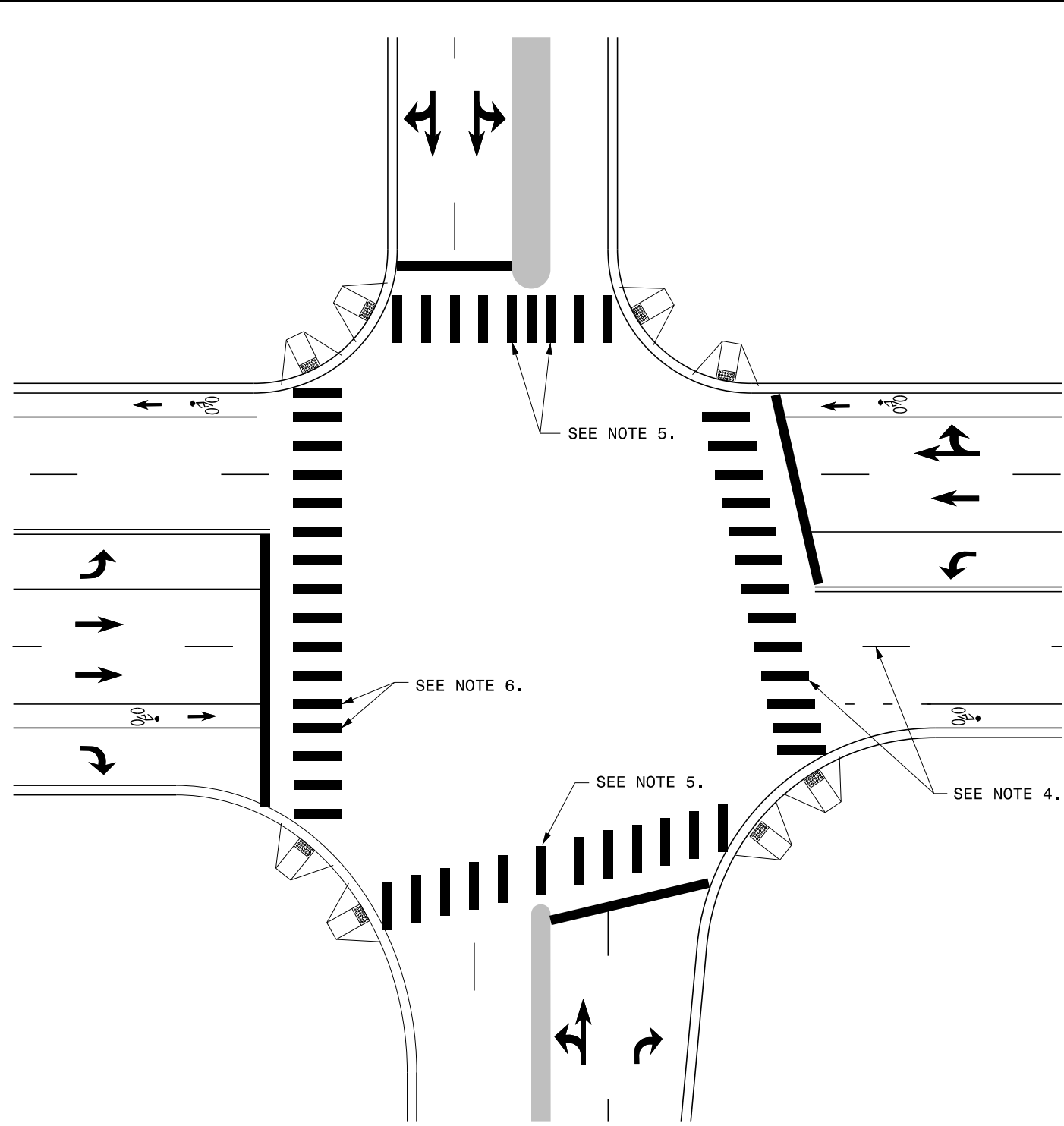
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ENGLISH TYPICAL DRAWING FOR  
**PAVEMENT MARKINGS**  
HI-VISIBILITY CROSSWALKS  
NO-TRACK MARKING GUIDANCE

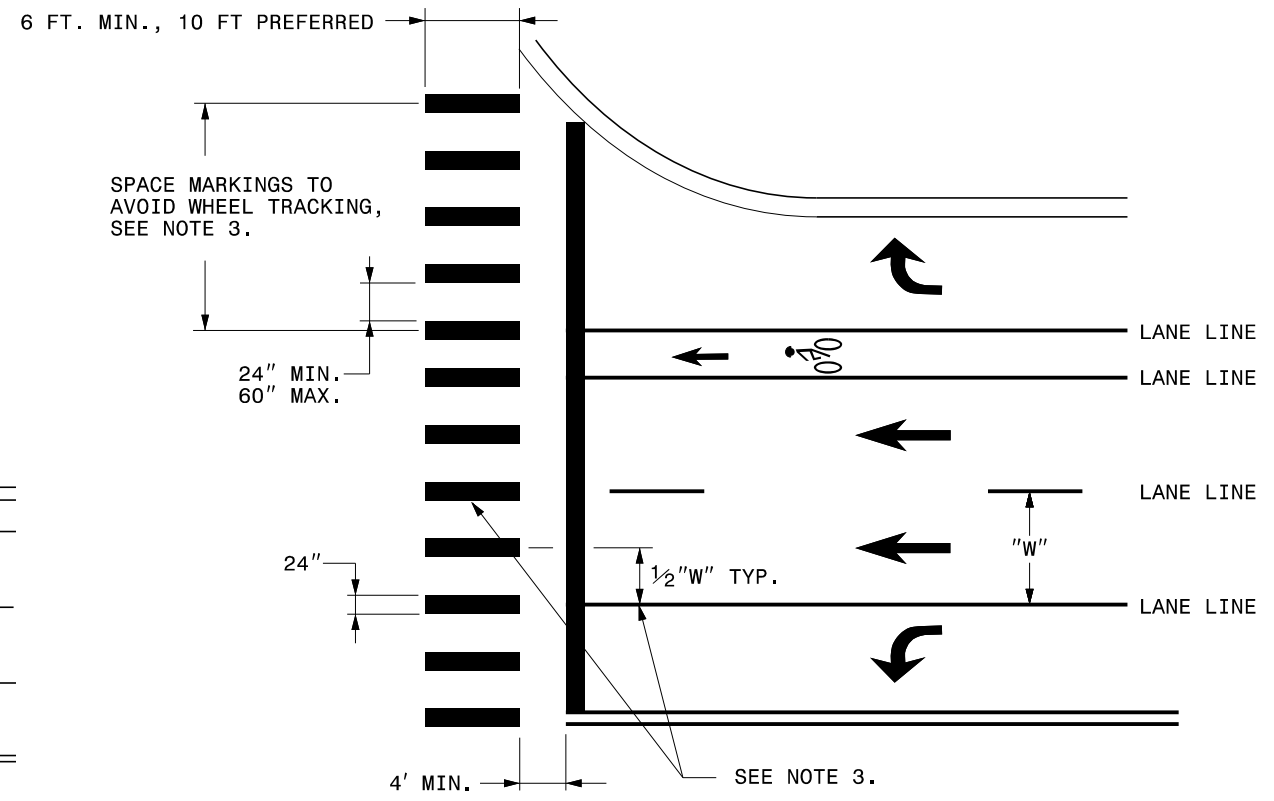
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DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

2-19

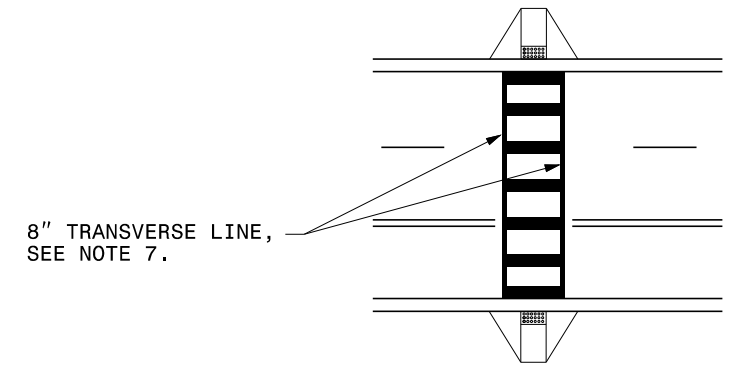
ENGLISH TYPICAL DRAWING FOR  
**PAVEMENT MARKINGS**  
HI-VISIBILITY CROSSWALKS  
NO-TRACK MARKING GUIDANCE



CROSSWALK INTERSECTION DETAIL



CROSSWALK MARKING DETAIL



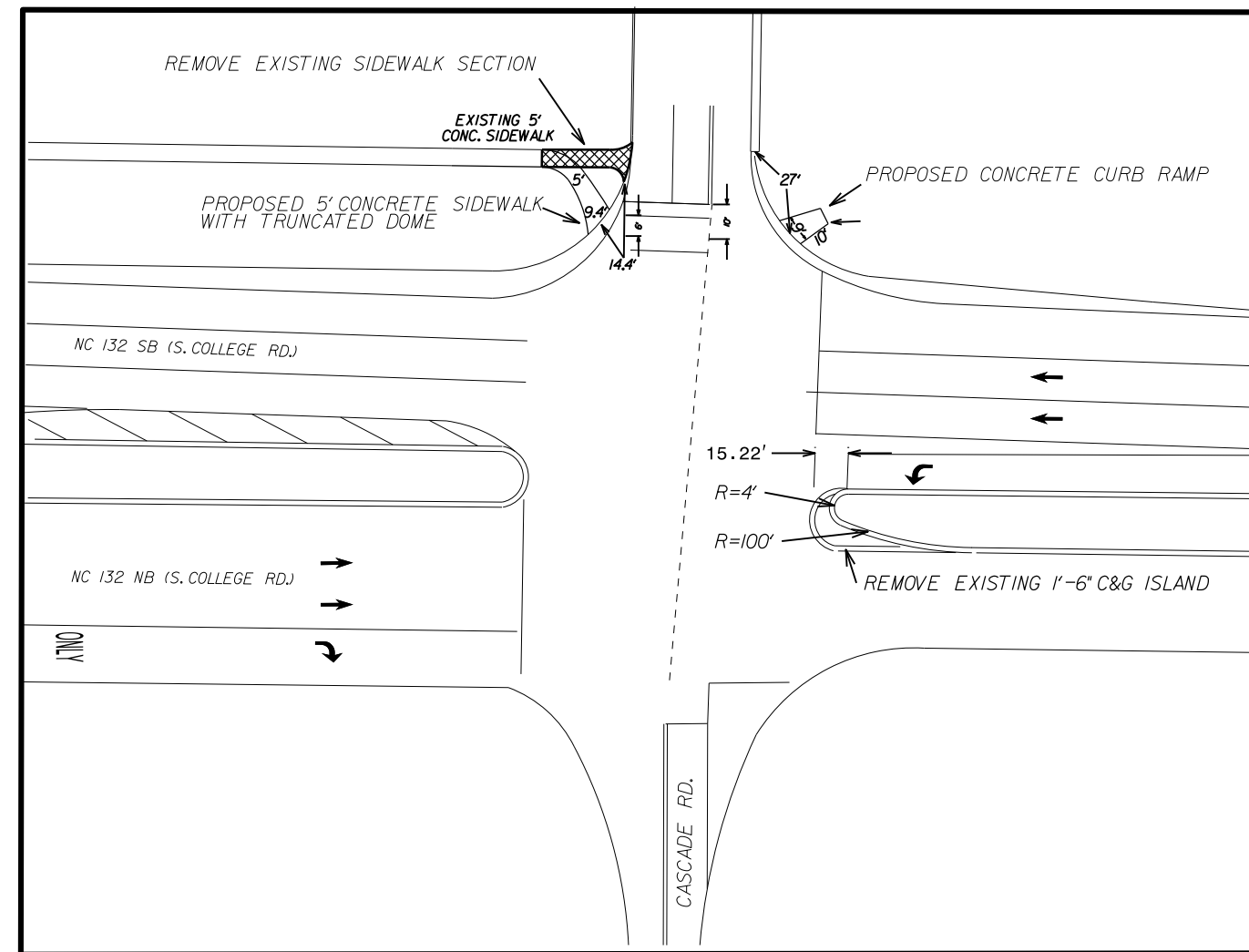
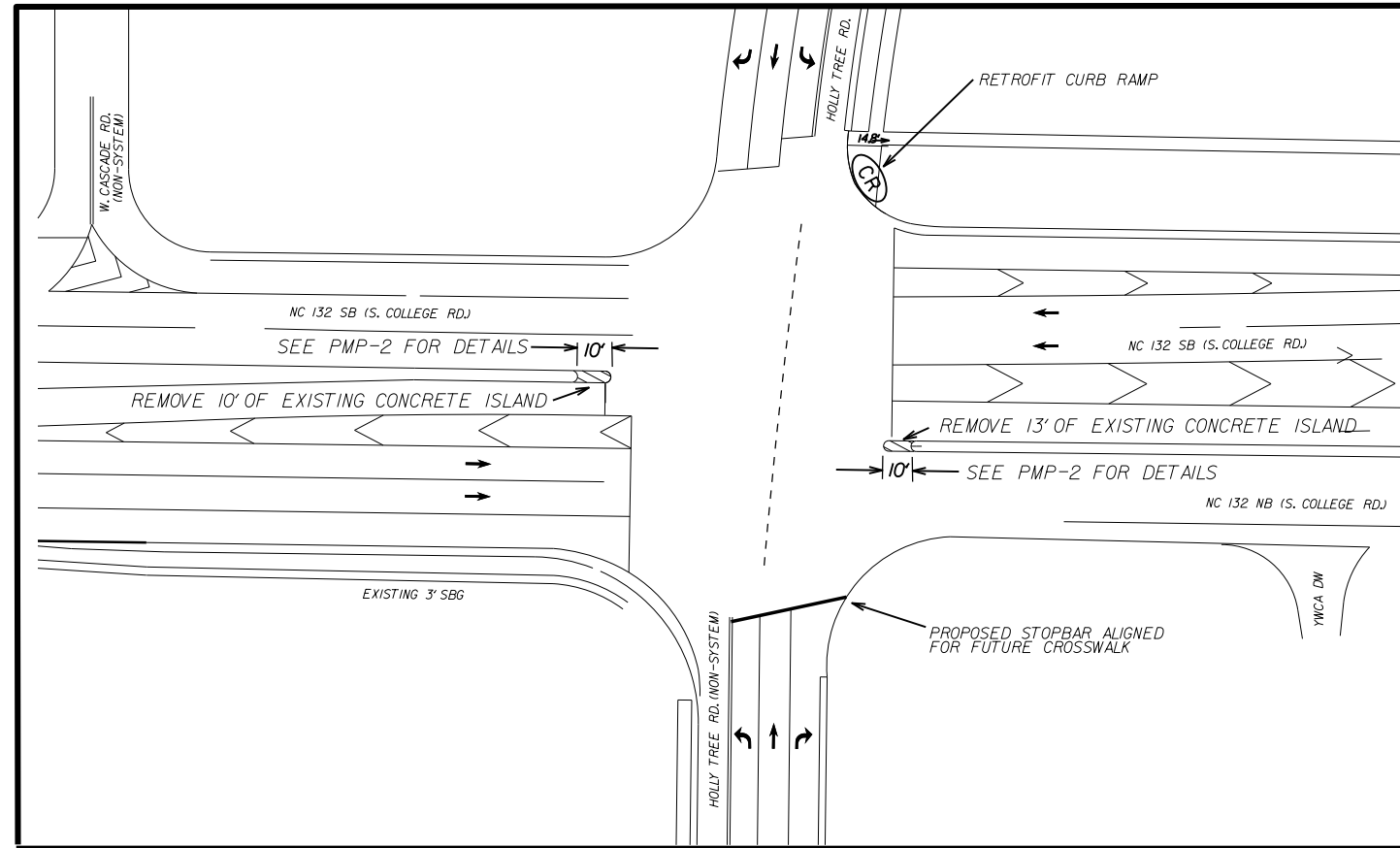
MID-BLOCK CROSSWALK DETAIL

GENERAL NOTES:

- 1- THE GUIDANCE IN THIS DETAIL IS TO BE USED WHEN NO-TRACK SPACING OF HI-VISIBILITY MARKINGS IS REQUESTED.
- 2- USE THE GUIDANCE SHOWN ON THE ABOVE DETAILS IN CONJUNCTION WITH PAVEMENT MARKING GUIDANCE SHOWN ON ROADWAY STANDARD DRAWINGS 1205.01 AND 1205.07.
- 3- PLACE MARKINGS TO AVOID WHEELPATH OF VEHICLES. MARKINGS TYPICALLY WILL BE LOCATED CENTERED AT THE LANE LINES AND EDGE LINES WITH ONE ADDITIONAL MARKING CENTERED IN THE MIDDLE OF THE LANE. AT WIDE LANE WIDTHS DUE TO TAPERS AND LARGE RADII, LOCATE MARKINGS AT BEST SPACING TO AVOID WHEEL TRACKING. THE SPACE BETWEEN MARKINGS SHALL NOT BE LESS THAN 24 INCHES OR GREATER THAN 60 INCHES.

- 4- WHERE THE CROSSWALK IS SKEWED TO THE LANE LINES, THE MARKINGS SHOULD BE PARALLEL TO THE LANE LINES.
- 5- PLACE MARKINGS ON BOTH EDGES OF THE NOSE OF A MEDIAN. FOR NARROW MEDIANS LESS THAN 4 FEET, A SINGLE MARKING MAY BE USED. FOR WIDE MEDIANS, INSTALL ADDITIONAL MARKINGS IN THE MEDIAN AREA. THE SPACE BETWEEN THE MARKINGS SHALL NOT TO BE LESS THAN 24 INCHES OR GREATER THAN 60 INCHES.
- 6- LOCATE MARKINGS CENTERED ON BICYCLE LANE LINES. MARKINGS SHALL NOT BE LOCATED IN THE CENTER OF THE BICYCLE PATH.
- 7- INCLUDE 8" TRANSVERSE LINES WITH 24" LONGITUDINAL MARKINGS AT MID-BLOCK CROSSINGS.

# INTERSECTION AT HOLLYTREE RD. & CASCADE RD. CONCRETE ISLAND, SIDEWALK, & CURB RAMP DETAIL SHEET



**MAPS N.T.S.**

08-MAP-2021-20-33  
 S:\Division\Resurfacing\Resurfacing Data\2021\Resurfacing\NEW HANDOVER\2021 NEW HANDOVER SPRING LET\2021CPT.03.17.10651.RdJ.txd.dgn  
 8/17/99  
 \$\$\$SERVERNAME\$\$\$



DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA

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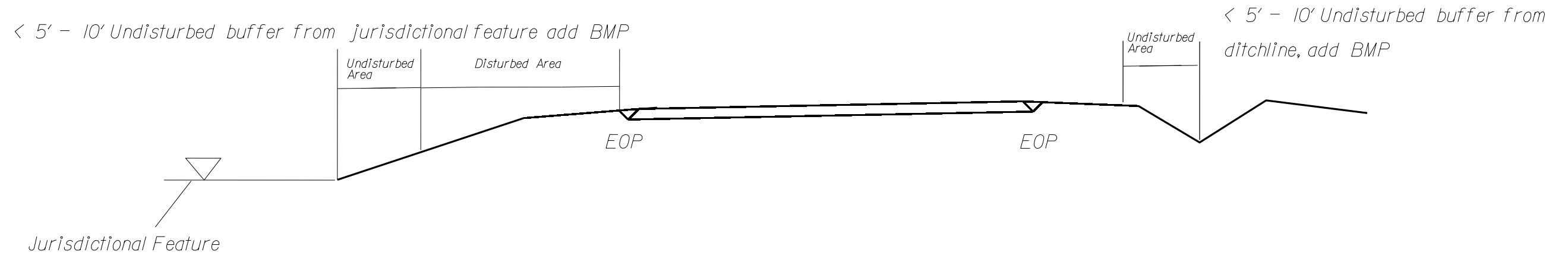
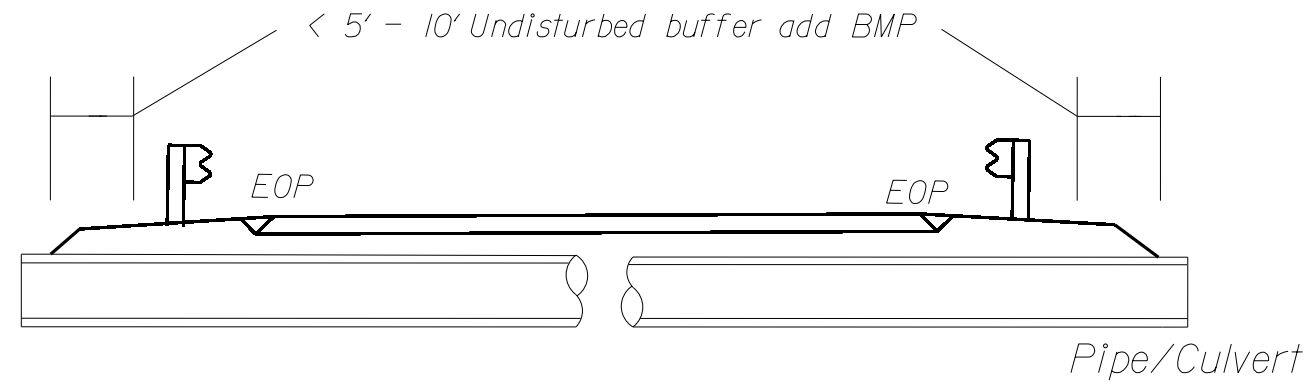
## ***SOIL STABILIZATION TIMEFRAMES***

<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

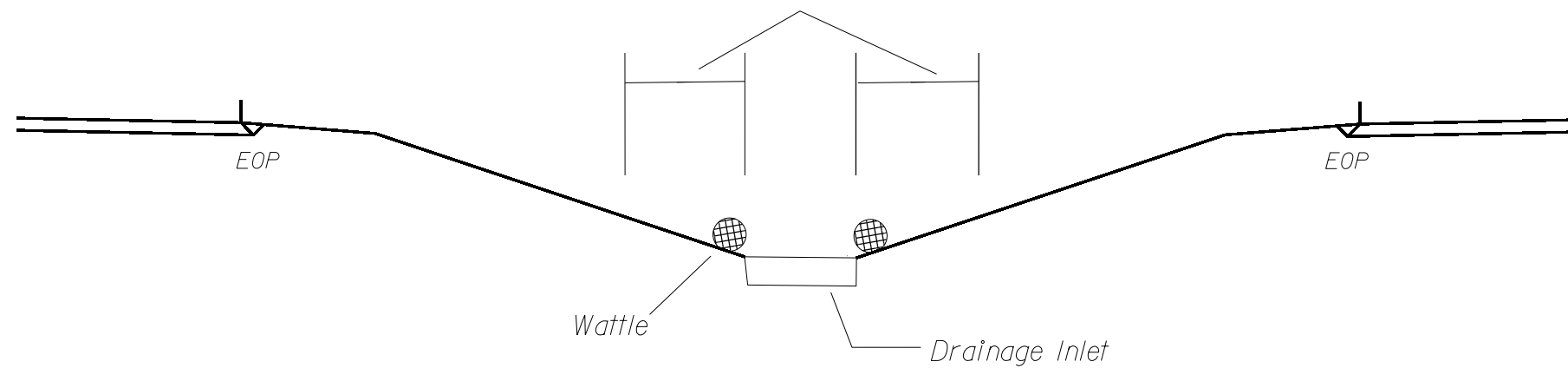
# EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

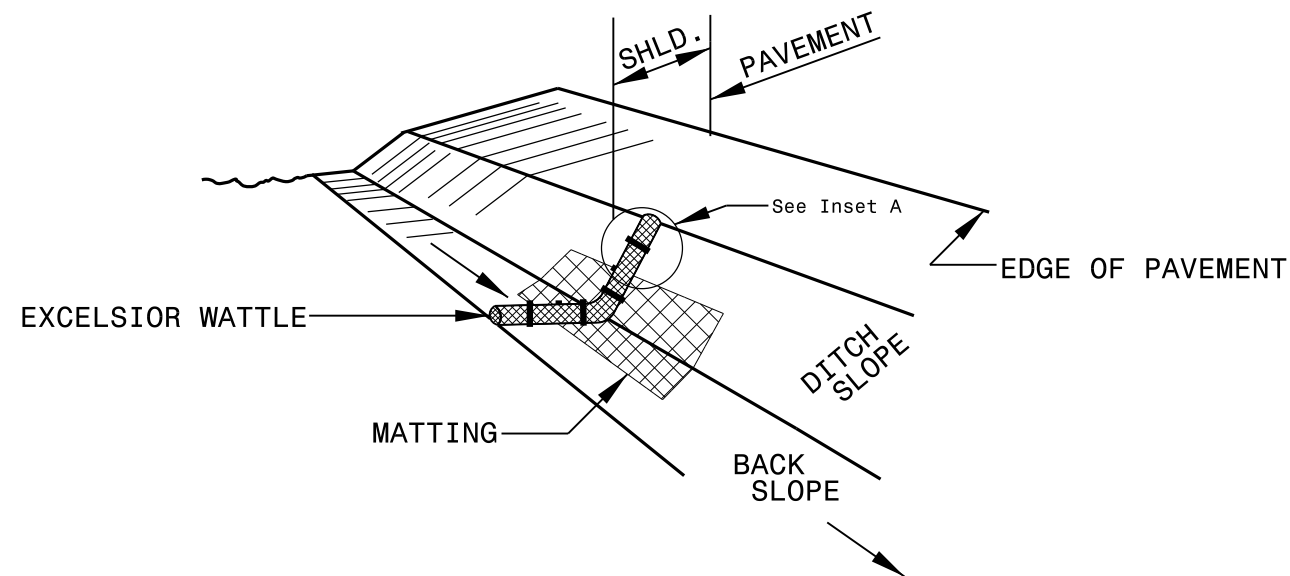


< 5' - 10' Undisturbed buffer from inlet, add wattle

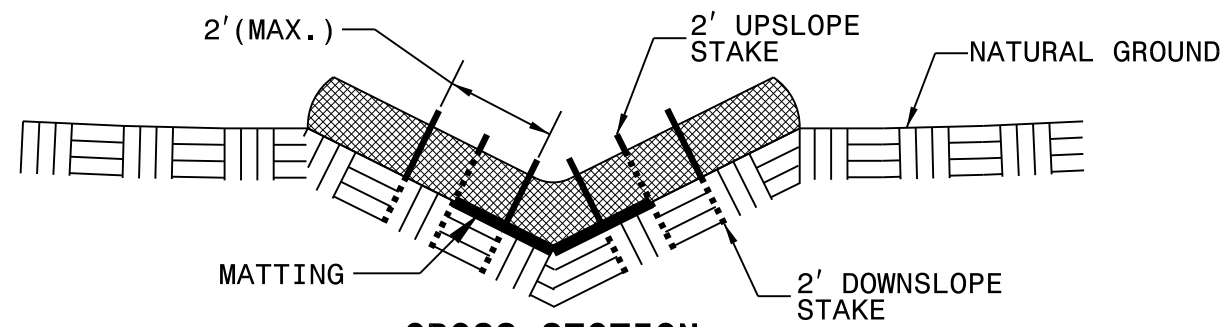


NOT TO SCALE

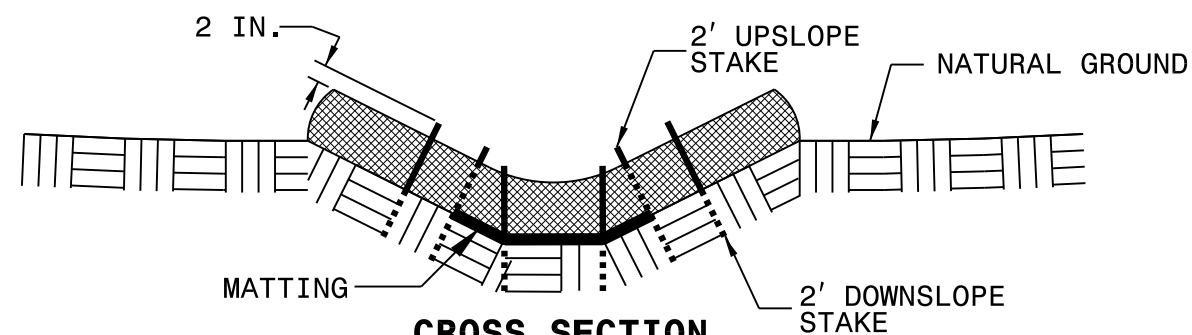
# WATTLE DETAIL



**ISOMETRIC VIEW**



**CROSS SECTION VEE DITCH**



**CROSS SECTION TRAPEZOIDAL DITCH**

**NOTES:**

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

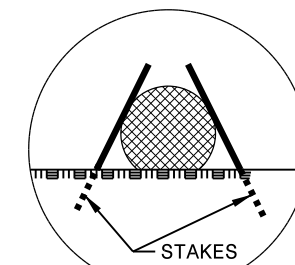
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

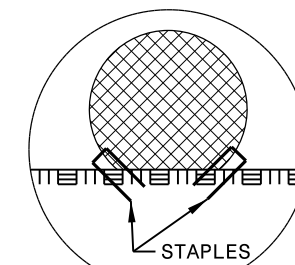
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

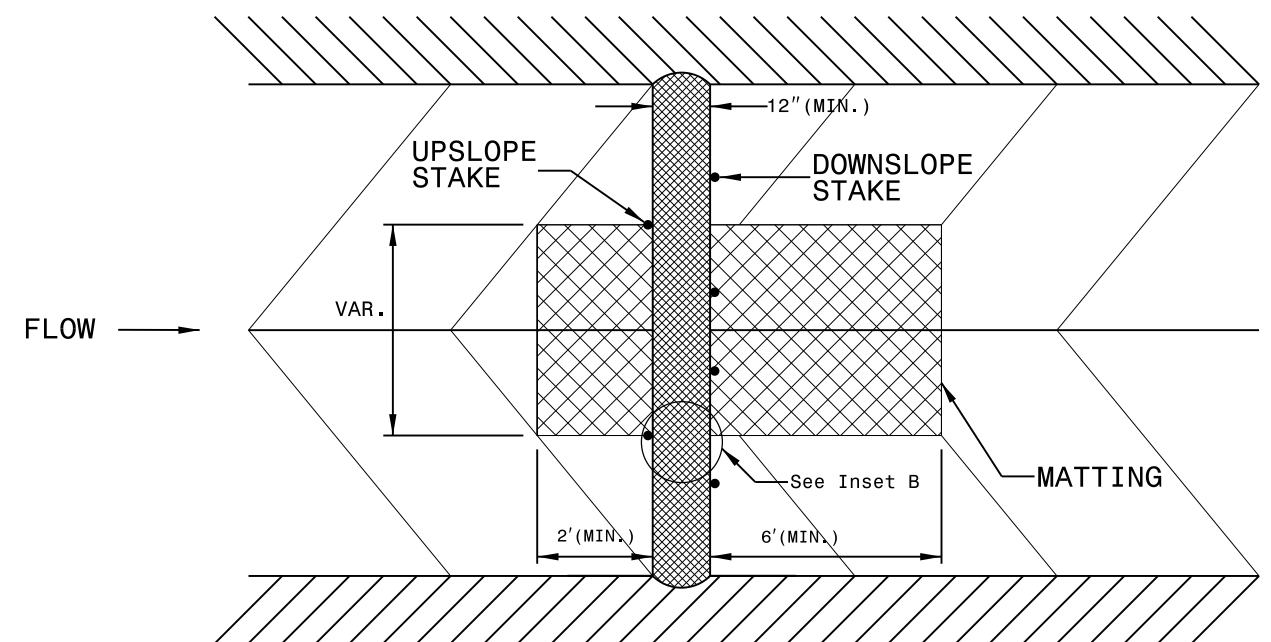
INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



**INSET A**



**INSET B**



**TOP VIEW**














**STATE OF NORTH CAROLINA  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN  
NEW HANOVER COUNTY**

**LOCATION: NC 132 (S. COLLEGE RD) STA. 10+.00 +/- TO STA. 34+72 +/-.**

<small>WBS NO.</small> 2021CPT.03.17.10651	<small>SHEET NO.</small> PMP-1
<small>DocuSigned by:</small> <i>Ayman Alqudwah</i>	
<small>APPROVED:</small>	
<small>DATE:</small> 2/16/2021	
	
<b>DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED</b>	

**CONTRACT: DC00302**

**ROADWAY STANDARD DRAWING**

THE FOLLOWING ROADWAY STANDARDS AS APPEAR IN "ROADWAY STANDARD DRAWINGS" - PROJECT SERVICES UNIT - N.C. DEPARTMENT OF TRANSPORTATION - RALEIGH, N.C., DATED JANUARY 2018 ARE APPLICABLE TO THIS PROJECT AND BY REFERENCE HEREBY ARE CONSIDERED A PART OF THESE PLANS:

<u>STD. NO.</u>	<u>TITLE</u>
1205.01	PAVEMENT MARKINGS - LINE TYPES AND OFFSETS
1205.02	PAVEMENT MARKINGS - TWO-LANE AND MULTILANE ROADWAYS
1205.04	PAVEMENT MARKINGS - INTERSECTIONS
1205.05	PAVEMENT MARKINGS - TURN LANES
1205.06	PAVEMENT MARKINGS - LANE DROPS
1205.07	PAVEMENT MARKINGS - PEDESTRIAN CROSSWALKS
1205.08	PAVEMENT MARKINGS - SYMBOLS AND WORD MESSAGES
1205.09	PAVEMENT MARKINGS - PAINTED ISLANDS
1250.01	RAISED PAVEMENT MARKERS - INSTALLATION SPACING
1251.01	RAISED PAVEMENT MARKERS - PERMANENT AND TEMPORARY
1253.01	RAISED PAVEMENT MARKERS - SNOWPLOWABLE

**PAVEMENT MARKING SCHEDULE**

- THERMOPLASTIC (4" 90 MIL)**
- T1 WHITE EDGELINE
  - T2 WHITE SOLID LANE LINE
  - T3 10' WHITE SKIP
  - T4 3FT.-9FT./SP WHITE MINISKIP
  - T10 YELLOW EDGELINE
  - T13 YELLOW DOUBLE CENTER
  - T14 2FT.-6FT./SP YELLOW MINISKIP
- THERMOPLASTIC (8" 90 MIL)**
- T40 WHITE GORE LINE
  - T41 WHITE DIAGONAL
  - T42 YELLOW DIAGONAL
  - T43 WHITE SOLID LANE
  - T44 3FT.-9FT./SP WHITE MINISKIP
  - T46 WHITE CROSSWALK LINE
- THERMOPLASTIC (24" 90 MIL)**
- T61 WHITE STOPBAR
- THERMOPLASTIC MARKING SYMBOLS (90 MIL)**
- T70 LEFT TURN ARROW
  - T71 RIGHT TURN ARROW
  - T72 STRAIGHT ARROW
  - T74 COMBO RIGHT/STRAIGHT ARROW
  - T76 COMBO LEFT/RIGHT/STRAIGHT ARROW
- THERMOPLASTIC MARKING CHARACTER (90 MIL)**
- T100 ALPHANUMERIC CHAR.
- SNOWPLOWABLE PAVEMENT MARKERS**
- ME YELLOW & YELLOW
  - MF CRYSTAL & RED

**GENERAL NOTES**

- THE FOLLOWING GENERAL NOTES APPLY AT ALL TIMES FOR THE DURATION OF THE CONSTRUCTION PROJECT, EXCEPT WHEN OTHERWISE NOTED IN THE PLAN, OR DIRECTED BY THE ENGINEER.
- A) INSTALL PAVEMENT MARKINGS AND PAVEMENT MARKERS ON THE FINAL SURFACE AS FOLLOWS:
 

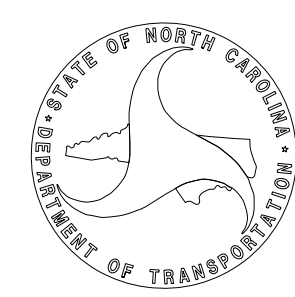
<small>ROAD NAME</small>	<small>MARKING</small>	<small>MARKER</small>
US 132 (COLLEGE RD)	THERMOPLASTIC	SNOWPLOWABLE
  - B) TIE PROPOSED PAVEMENT MARKING LINES TO EXISTING PAVEMENT MARKING LINES.
  - C) REMOVE/REPLACE ANY CONFLICTING/DAMAGED PAVEMENT MARKINGS AND MARKERS.
  - D) PASSING ZONES WILL BE DETERMINED IN THE FIELD AND MUST BE APPROVED BY THE ENGINEER.
  - E) STOP BAR LOCATION AT NON-SIGNALIZED INTERSECTIONS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER.

**INDEX**

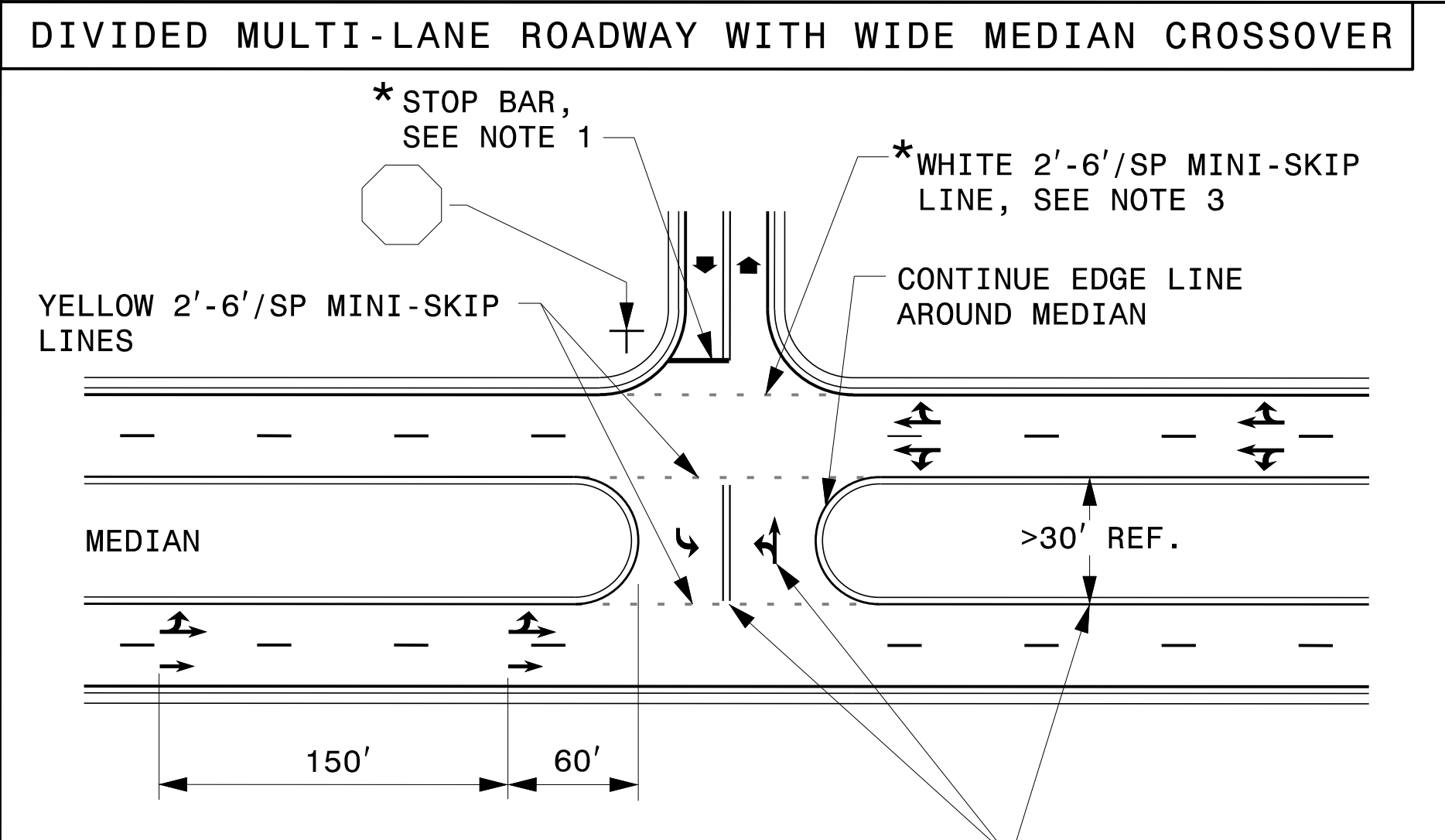
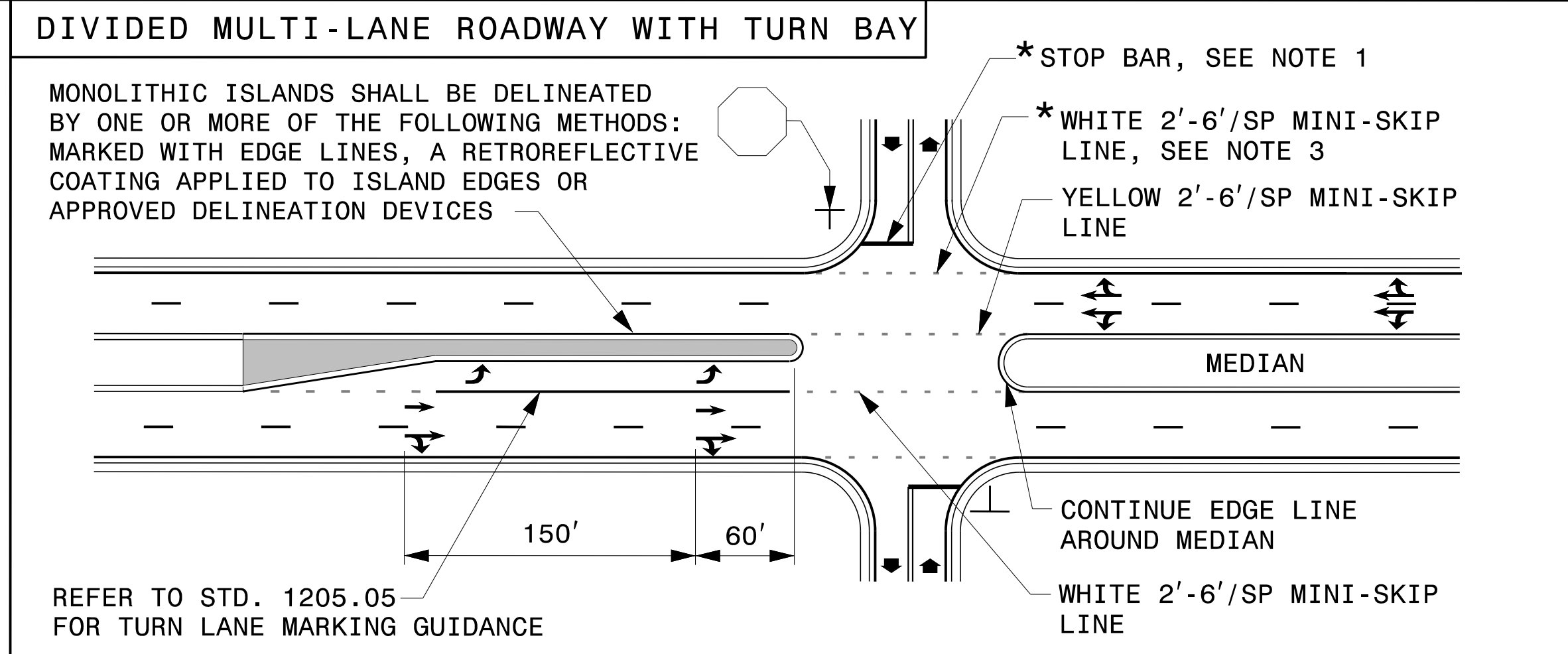
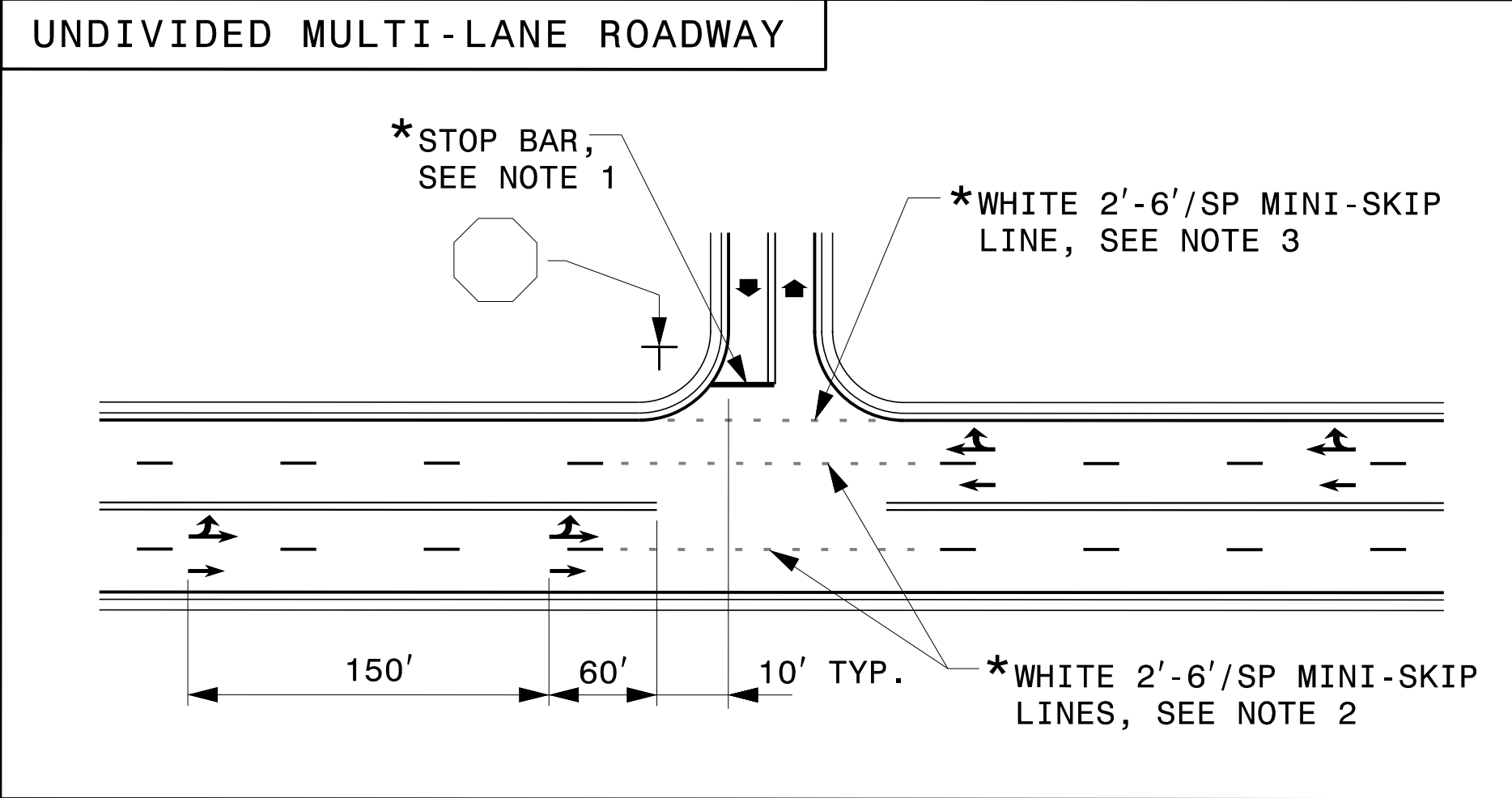
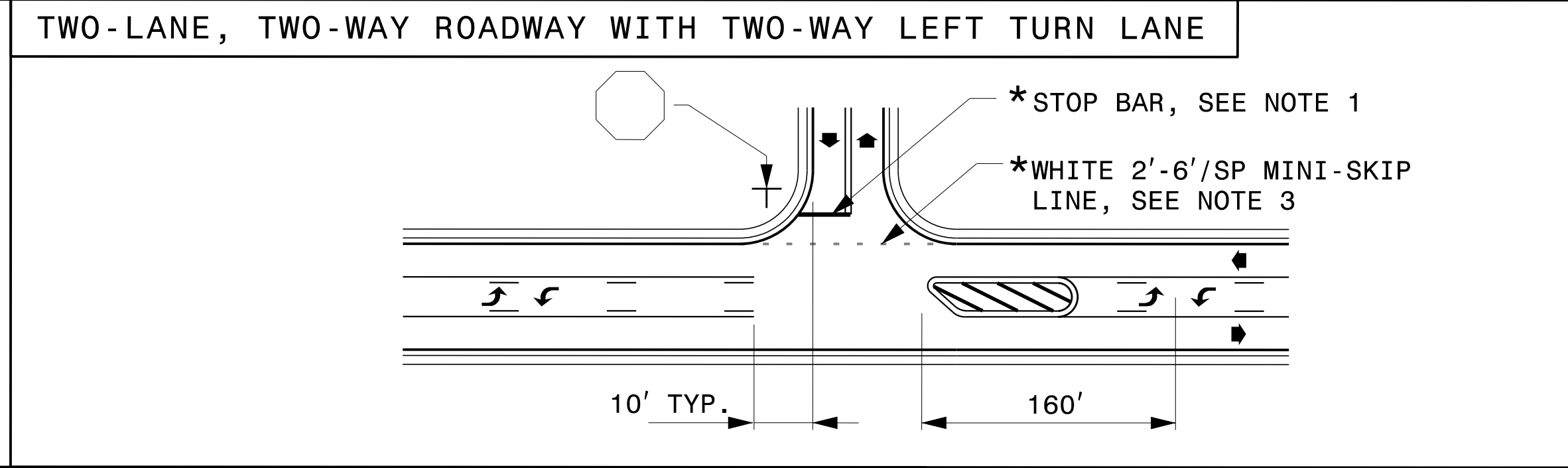
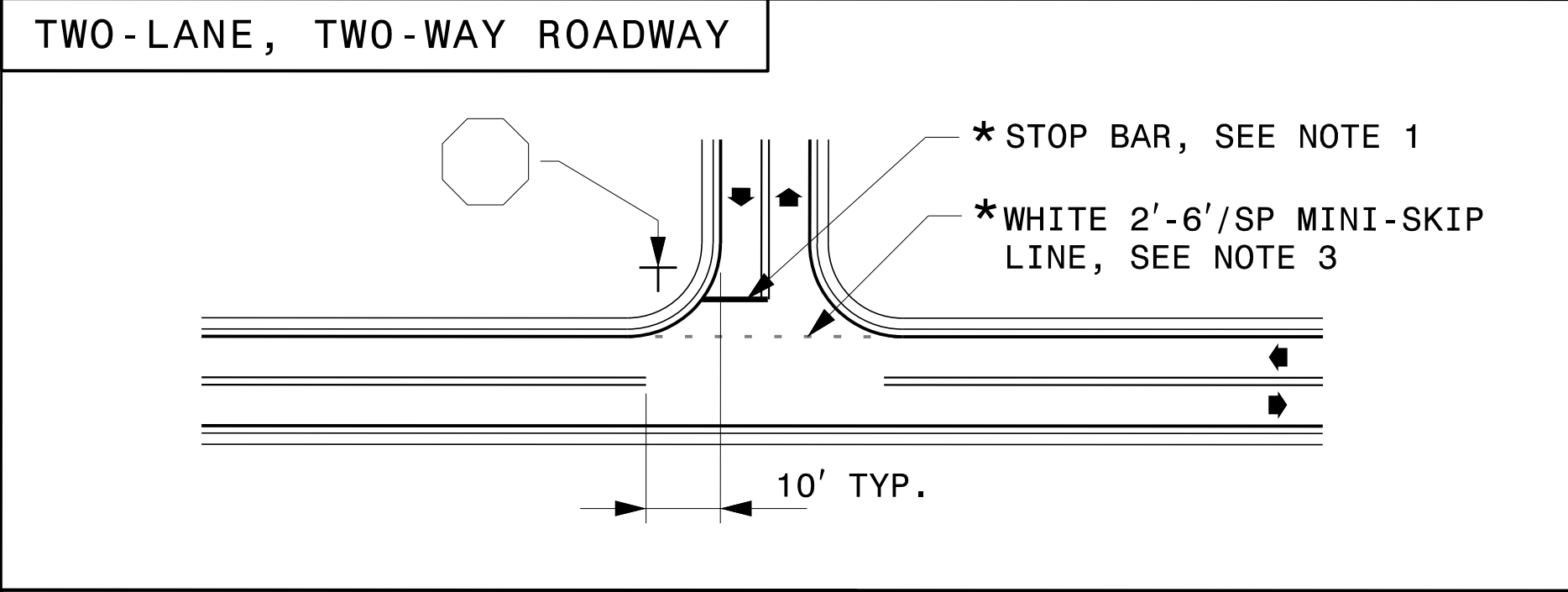
<u>SHEET NO.</u>	<u>DESCRIPTION</u>
PMP-1	PAVEMENT MARKING PLAN TITLE AND SCHEDULE SHEET
PMP-1A	REVISED PAVEMENT MARKING ROADWAY STANDARD DRAWING
PMP-2-3	PAVEMENT MARKING DETAIL

PLAN PREPARED BY: N.C.D.O.T. SIGNING AND DELINEATION UNIT

AYMAN ALQUDWAH, P.E. SIGNING & DELINEATION REGIONAL ENGINEER  
STACEY JOHNS SIGNING & DELINEATION PROJECT DESIGN ENGINEER







STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.



**GENERAL NOTES:**

- 1- PLACEMENT OF STOP BARS AT NON-SIGNALIZED INTERSECTIONS IS OPTIONAL AND USED WHERE IT IS IMPORTANT TO INDICATE THE POINT WHICH VEHICLES ARE REQUIRED TO STOP. PLACE STOP BARS NO LESS THAN 4 FEET AND NO MORE THAN 30 FEET FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY. USE 10 FEET AS THE TYPICAL SETBACK DISTANCE OR AS DIRECTED BY THE ENGINEER.
- 2- MINI-SKIP LANE LINE EXTENSIONS SHOULD BE USED AT INTERSECTIONS THAT HAVE REDUCED VISIBILITY CONDITIONS SUCH AS OFFSET, SKEWED, OR CURVED ROADWAYS.
- 3- MINI-SKIP EDGE LINE EXTENSIONS MAY BE PLACED THROUGH INTERSECTIONS AND MAJOR DRIVEWAYS.
- 4- REFER TO ROADWAY STANDARD DRAWINGS 1205.01, 1205.02, 1205.05, 1205.08 AND 1205.09 FOR ADDITIONAL PAVEMENT MARKING GUIDANCE.

LEGEND	
	STOP SIGN
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW
	PAVEMENT MARKING SYMBOLS
*	OPTIONAL

USE DOUBLE YELLOW CENTER LINE AND ARROW SYMBOLS IN MEDIAN CROSSOVER WHEN WIDTH OF MEDIAN EXCEEDS 30 FT, OTHERWISE THEY ARE NOT REQUIRED.

SHEET 2 OF 2  
**1205D04**

SHEET 2 OF 2  
**1205D04**

ENGLISH DETAIL DRAWING FOR  
**PAVEMENT MARKINGS**  
INTERSECTIONS

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**PAVEMENT MARKINGS**  
INTERSECTIONS

**REVISED PAVEMENT MARKING ROADWAY STANDARD DRAWING**

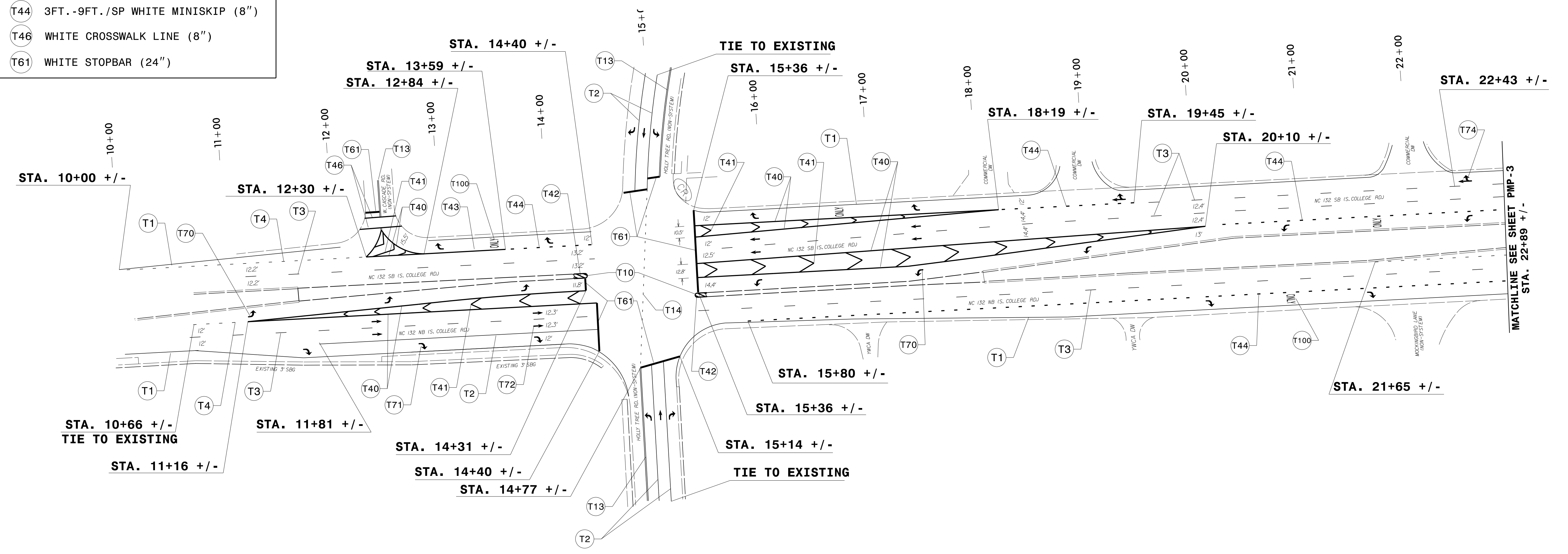
APPROVED: *Ayman Alqudwah*  
 DATE: 2/16/2021



DOCUMENT NOT CONSIDERED FINAL UNLESS ALL SIGNATURES COMPLETED

SYMBOL AND PAVEMENT MARKING LEGEND	
THERMOPLASTIC	
T1	WHITE EDGELINE (4")
T2	WHITE SOLID LANE LINE (4")
T3	10' WHITE SKIP (4")
T4	3FT.-9FT./SP WHITE MINISKIP (4")
T10	YELLOW EDGE LINE (4")
T13	YELLOW DOUBLE CENTER (4")
T14	2FT.-6FT./SP YELLOW MINISKIP (4")
T40	WHITE GORE LINE (8")
T41	WHITE DIAGONAL (8")
T42	YELLOW DIAGONAL (8")
T43	WHITE SOLID LANE LINE (8")
T44	3FT.-9FT./SP WHITE MINISKIP (8")
T46	WHITE CROSSWALK LINE (8")
T61	WHITE STOPBAR (24")

T70	LEFT TURN ARROW
T71	RIGHT TURN ARROW
T72	STRAIGHT ARROW
T74	COMBO RIGHT/STRAIGHT ARROW
T76	COMBO LEFT/RIGHT/STRAIGHT ARROW
ONLY T100	ALPHANUMERIC CHARACTER



02/16/21  
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 User:FSUJHNS

**PAVEMENT MARKING DETAIL**



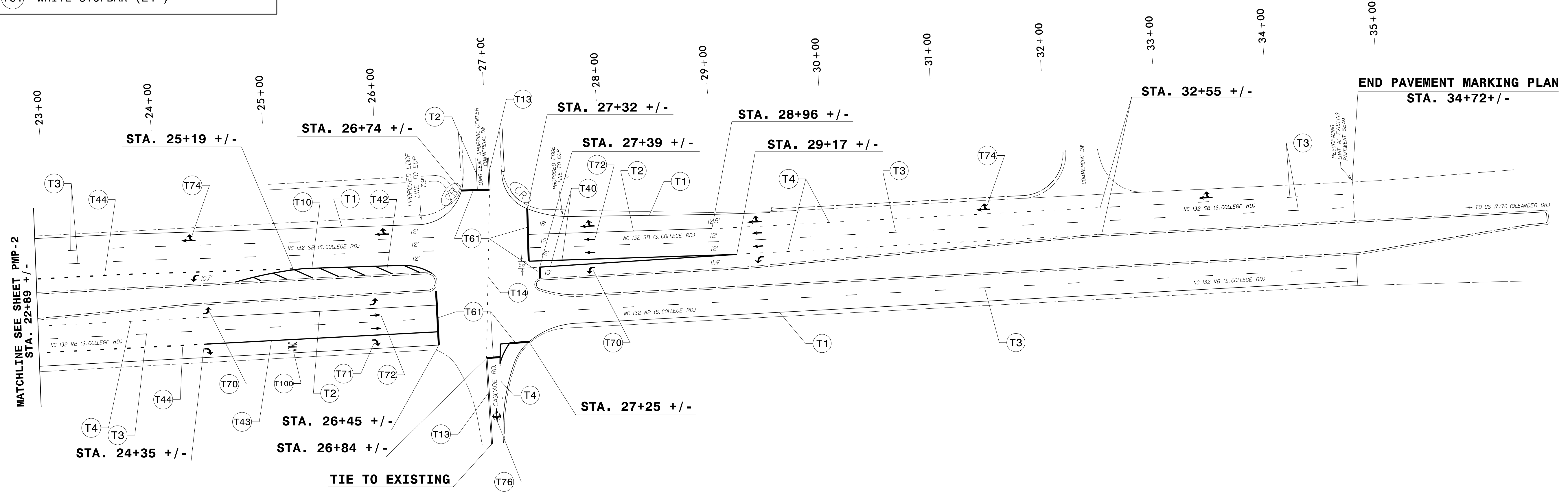
**DOCUMENT NOT CONSIDERED FINAL  
 UNLESS ALL SIGNATURES COMPLETED**

**SYMBOL AND  
 PAVEMENT MARKING LEGEND**

THERMOPLASTIC

- (T1) WHITE EDGELINE (4")
- (T2) WHITE SOLID LANE LINE (4")
- (T3) 10' WHITE SKIP (4")
- (T4) 3FT.-9FT./SP WHITE MINISKIP (4")
- (T10) YELLOW EDGE LINE (4")
- (T13) YELLOW DOUBLE CENTER (4")
- (T14) 2FT.-6FT./SP YELLOW MINISKIP (4")
- (T40) WHITE GORE LINE (8")
- (T41) WHITE DIAGONAL (8")
- (T42) YELLOW DIAGONAL (8")
- (T43) WHITE SOLID LANE LINE (8")
- (T44) 3FT.-9FT./SP WHITE MINISKIP (8")
- (T46) WHITE CROSSWALK LINE (8")
- (T61) WHITE STOPBAR (24")

- ↶ (T70) LEFT TURN ARROW
- ↷ (T71) RIGHT TURN ARROW
- (T72) STRAIGHT ARROW
- ↶↷ (T74) COMBO RIGHT/STRAIGHT ARROW
- ↷↶ (T76) COMBO LEFT/RIGHT/STRAIGHT ARROW
- ONLY (T100) ALPHANUMERIC CHARACTER



02/16/21  
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 User:FSUJHNS

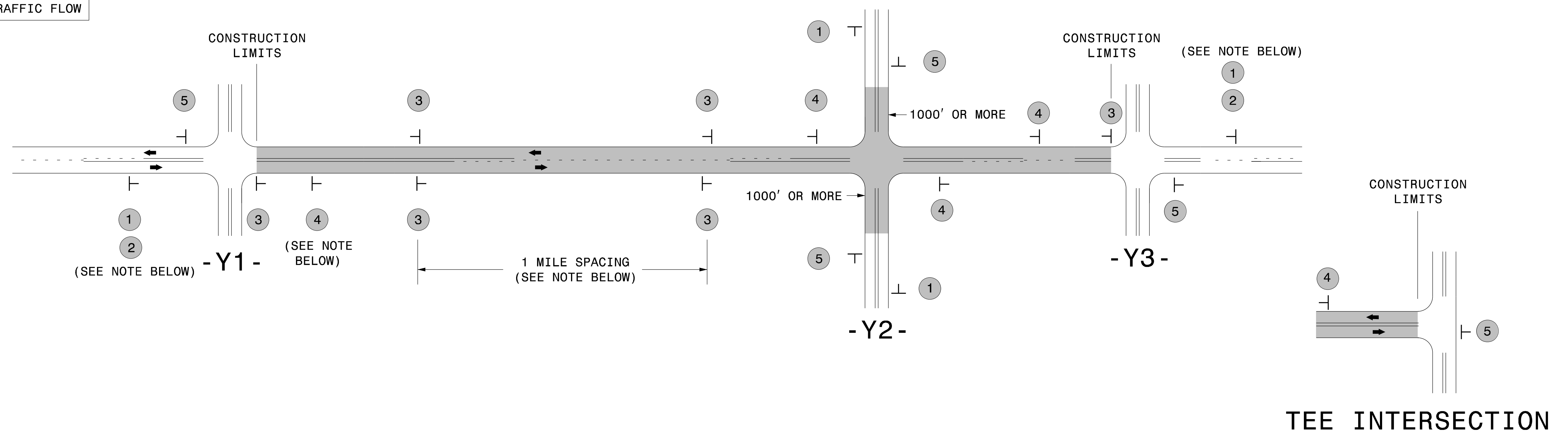
**PAVEMENT MARKING DETAIL**

# SIGNING FOR RESURFACING PROJECTS

**LEGEND**

┃ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



## MAINLINE (-L-) SIGNING

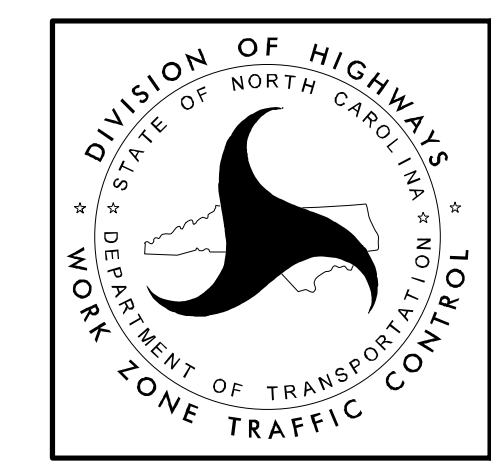
## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	 W20-1 48" X 48"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:  1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS  WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.  <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">                       W20-1                      48" X 48"                      PLACED 500' IN ADVANCE OF FLAGGER.                 </div> <div style="text-align: center;">                       W20-7 A                      48" X 48"                      PLACED 250' IN ADVANCE OF FLAGGER.                 </div> </div>
	2	 W7-3aP 24" X 18"	#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3	 SP 13107 48" X 48"	- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4	 SP 13106 48" X 48"	- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

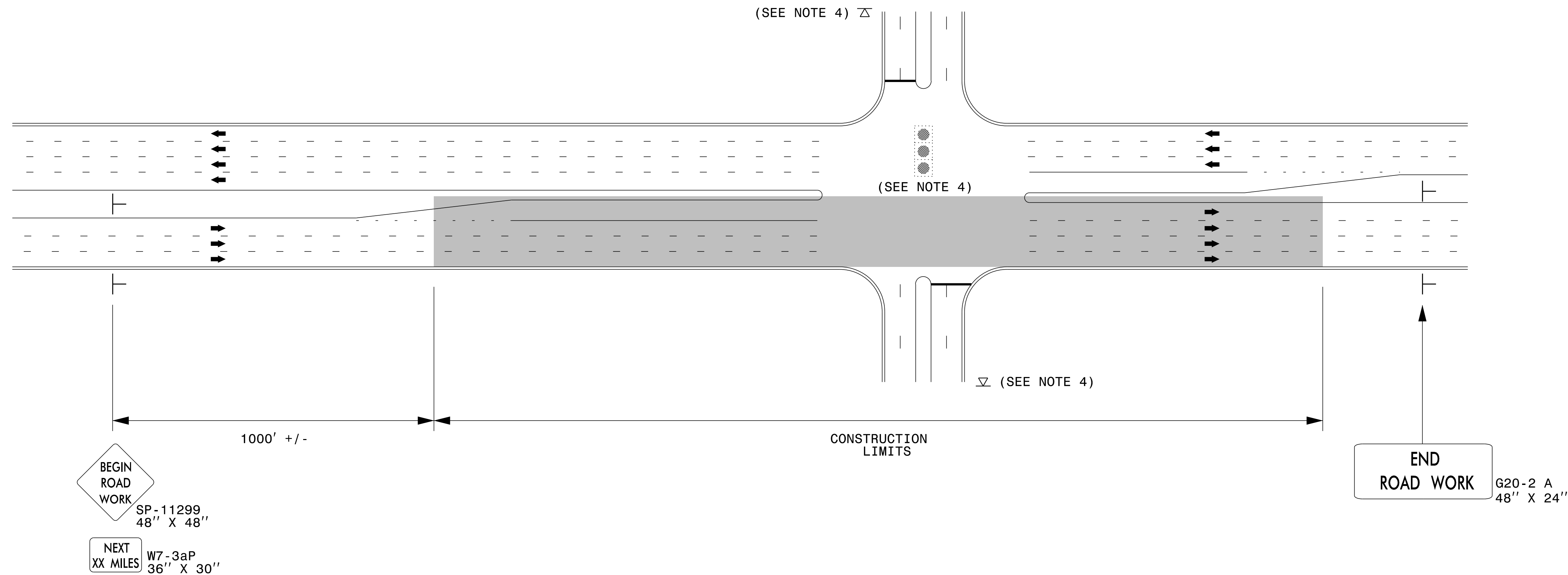
## MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

## URBAN / SUBURBAN WORKZONES

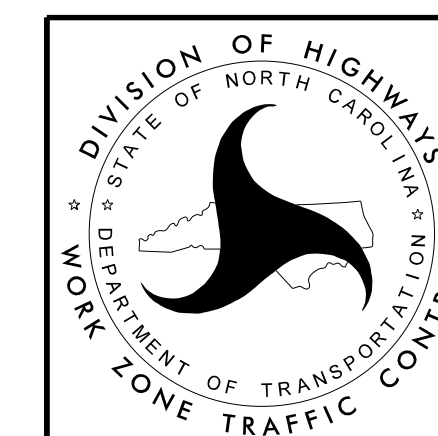


### NOTES:

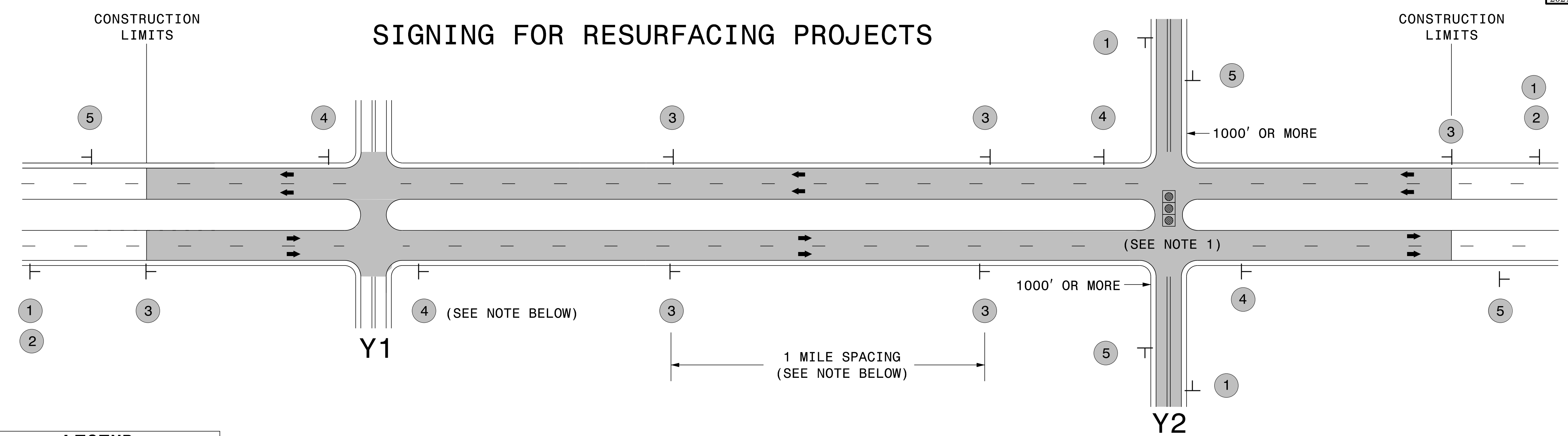
- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

### LEGEND

- ┆ STATIONARY SIGN
- ➔ DIRECTION OF TRAFFIC FLOW



**RESURFACING ADVANCE  
WARNING SIGNS FOR  
URBAN / SUBURBAN  
FACILITIES**



**LEGEND**  
 ┆ STATIONARY SIGN  
 ← DIRECTION OF TRAFFIC FLOW

**MAINLINE (-L-) SIGNING**

**-Y- LINE SIGNING**

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	 W20-1 48" X 48"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p><b>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;">           W20-1          48" X 48"       </div> <div style="text-align: center;">           W20-7 A          48" X 48"       </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p><b>NOTES:</b></p> <ol style="list-style-type: none"> <li>1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.</li> </ol>
	2	 W7-3aP 24" X 18"	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3	 SP 13107 48" X 48"	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	
	4	 SP 13106 48" X 48"	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.	
5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.		

**RESURFACING  
 ADVANCE WARNING SIGNS  
 FOR RURAL AND SUBURBAN  
 MULTI-LANE ROADWAYS  
 W/ SHOULDER SECTIONS**